



Allyson Foster Boyd Allyson Foster Boyd
2025.07.16 11:47:45-04'00

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CDM Smith
1100 Marion Street
Suite 300
Knoxville, TN 37921

Allyson Foster Boyd, P.E. No. 119,736

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	BRIDGE-SIGN1
TITLE SHEET	1
INDEX AND STANDARD DRAWINGS	1A
PROJECT COMMITMENTS.....	1B
ESTIMATED ROADWAY QUANTITIES & UTILITY OWNERS	2A
GENERAL NOTES AND SPECIAL NOTES.....	2B – 2B1
GUARDRAIL PLAN AND PAVEMENT TRANSITION DETAILS WESTFIELD DR	2C
TRAFFIC CONTROL PLANS BR. NO. 82-B357-0.05 OVER I-26 PHASE 1.....	3
TRAFFIC CONTROL PLANS BR. NO. 82-B357-0.05 OVER I-26 PHASE 2.....	4
TRAFFIC CONTROL PLANS BR. NO. 82-B357-0.05 OVER I-26 DETOUR PLAN – VICTORY LN	5
TRAFFIC CONTROL PLANS (BR. NO. 82-B357-0.05) I-26 PHASE 1	6 – 6N
TRAFFIC CONTROL PLANS (BR. NO. 82-B357-0.05) I-26 PHASE 2	7 – 7N



Susanne Dawson	Susanne Dawson 2025.07.16 12:07:19-04'00'
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CDM Smith
1100 Marion Street
Suite 300
Knoxville, TN 37921

Susanne Dawson, P.E. No. 111,826

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	BRIDGE-SIGN1
TITLE SHEET	1
LAYOUT OF BRIDGE TO BE REPAIRED	BR-133-268
GENERAL NOTES AND ESTIMATED BRIDGE QUANTITIES	BR-133-269
PHASE CONSTRUCTION	BR-133-270
SUPERSTRUCTURE REPAIRS	BR-133-271
BEAM REPAIRS	BR-133-272
ABUTMENT REPAIRS	BR-133-273
BENT 1 REPAIRS	BR-133-274
BENT 2 REPAIRS	BR-133-275
BENT 3 REPAIRS	BR-133-276
JOINT REPLACEMENT AND EPOXY OVERLAY DETAILS	BR-133-277
CONCRETE REPAIR DETAILS	BR-133-278

YEAR	CONST. NO.	SHEET NO.
2025	82I026-M3-003	BRIDGE-SIGN1
SULLIVAN CO.		WESTFIELD DR.
STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION		
SIGNATURE SHEET		
BRIDGE NO. 82-0B357-0.05 FEDERAL BRIDGE ID NO. 82I00260013 WESTFIELD DRIVE OVER I-26 SULLIVAN COUNTY 2025		

INDEX OF SHEETS

SEE SHEET 1A FOR
INDEX AND STANDARD DRAWINGS

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

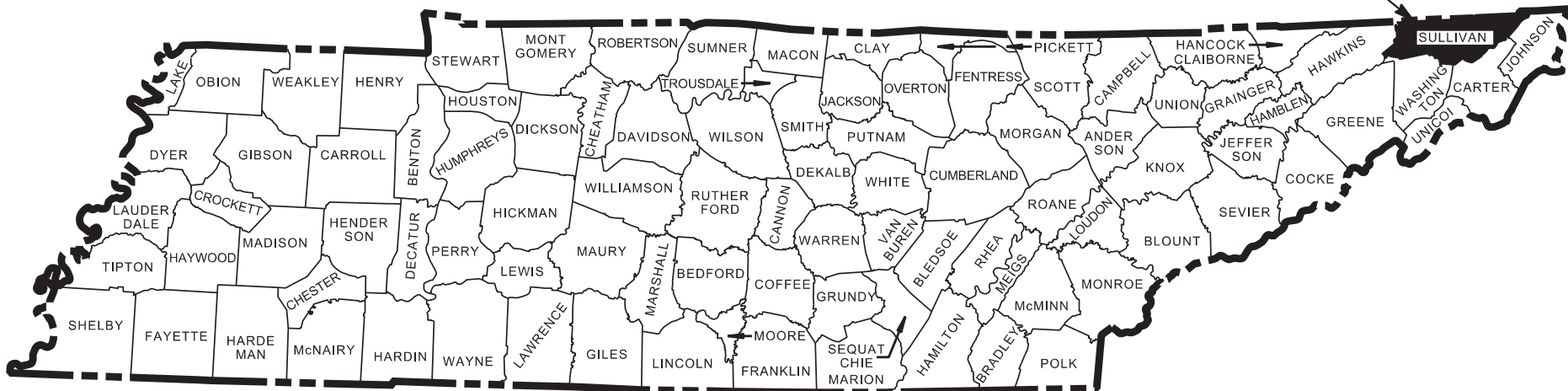
SULLIVAN COUNTY

WESTFIELD DRIVE BRIDGE OVER I-26
LM 0.05

PS&E

BRIDGE REPAIR

PROJECT LOCATION

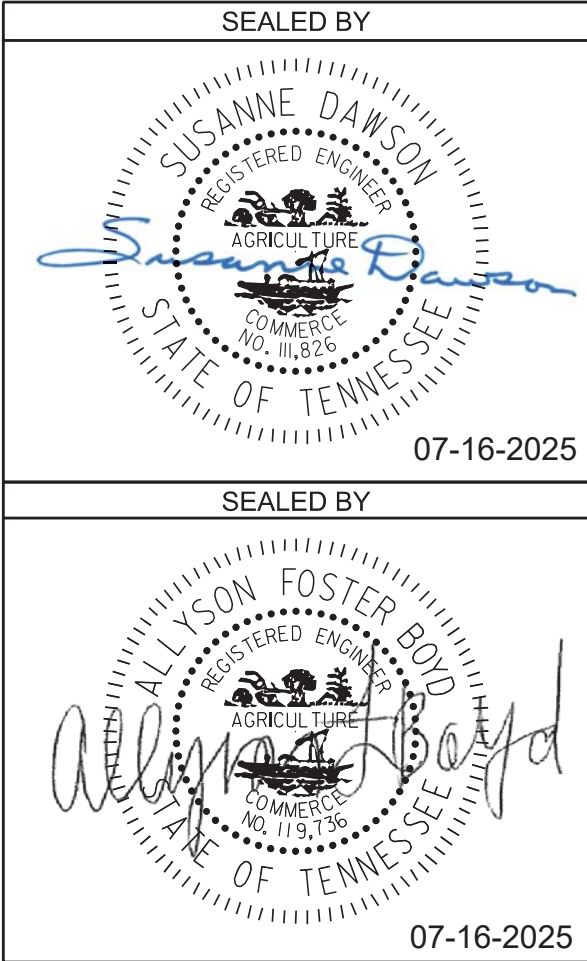


SULLIVAN CO.

BRIDGE ID. # 82-0B357-0.05



PS&E
PLANS



INTERSTATE 26 TRAFFIC DATA	
ADT (2025)	47,233
ADT (2045)	58,146
DHV (2025)	4,723
D	51-49
T (ADT)	7%
T (DHV)	7%
V	65 MPH

APPROVED:
WILL REID, CHIEF ENGINEER

DATE: _____

APPROVED:
WILL REID, COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: _____
DIVISION ADMINISTRATOR

DATE: _____

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT DESIGN MANAGER : STEPHEN WILSON
DESIGNED BY : CDM Smith
DESIGNER : SUSANNE DAWSON, P.E.
P.E. NO. 821026-M3-003
PIN NO. 134991.00

TOTAL DISTURBED AREA = LESS THAN AN ACRE

ROADWAY INDEX

SHEET NAME

SHEET NO.

SIGNATURE SHEET	BRIDGE-SIGN1
TITLE SHEET.....	1
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TRAFFIC CONTROL PLANS (BR. NO. 82-B357-0.05) I-26 PHASE 2	7 – 7N

NOTE: THE ALPHABETICAL LETTER, “I” IS NOT USED IN THE NUMBERING OF SHEETS.

STANDARD TRAFFIC DESIGN DRAWINGS

DWG. REV. DESCRIPTION

ROADWAY DESIGN STANDARDS

RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND

SAFETY DESIGN AND GUARDRAILS

S-CC-1	10-01-24	CRASH CUSHION
S-GR31-1A	06-28-19	GUARDRAIL AND BLOCK-OUT DETAILS

DESIGN - TRAFFIC CONTROL

T-WZ-10	03-26-25	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-12	03-26-25	ONE LANE CLOSURE DETAIL FOR BRIDGES ON DIVIDED HIGHWAYS
T-WZ-16	03-26-25	LANE SHIFT FOR DIVIDED HIGHWAYS AND FREEWAYS
T-WZ-21	03-26-25	LANE CLOSURE WITH LEFT HAND MERGE AND LANE SHIFT
T-WZ-32	03-26-25	TRAFFIC CONTROL PLAN SIGNAL LAYOUT FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-33	03-26-25	TRAFFIC CONTROL PLAN FOR CLOSE INTERSECTION CONDITIONS USING TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-34	03-26-25	TRAFFIC CONTROL PLAN GENERAL NOTES FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-35	03-26-25	TRAFFIC CONTROL PLAN PAY ITEM AND SIGN DETAILS FOR TRAFFIC SIGNAL AT TWO LANE BRIDGE RECONSTRUCTION SITE
T-WZ-63	03-26-25	WORK ZONE IN THE VICINITY OF AN EXIT RAMP
T-WZ-FAB1	03-26-25	FLASHING YELLOW ARROW BOARD
T-WZ-PBR1	03-26-25	INTERCONNECTED PORTABLE BARRIER RAIL
T-WZ-PBR2	03-26-25	DETAILS FOR WORK ZONE CHANNELIZATION DEVICES
T-WZ-PCB1	03-26-25	10 FOOT PORTABLE CONCRETE BARRIER RAIL
T-WZ-PCB2	03-26-25	20 FOOT PORTABLE CONCRETE BARRIER RAIL
T-WZ-PCB3	03-26-25	PORTABLE CONCRETE BARRIER RAIL DETAILS
T-WZ-PCB4	03-26-25	PORTABLE CONCRETE BARRIER RAIL ANCHOR PIN DETAILS

LIST OF BRIDGE DRAWINGS

DRAWING	SHT. NO.	DWG. NO.
LAYOUT OF BRIDGE TO BE REPAIRED	B-01	BR-133-268
GENERAL NOTES AND ESTIMATED BRIDGE QUANTITIES	B-02	BR-133-269
PHASE CONSTRUCTION	B-03	BR-133-270
SUPERSTRUCTURE REPAIRS	B-04	BR-133-271
BEAM REPAIRS	B-05	BR-133-272
ABUTMENT REPAIRS	B-06	BR-133-273
BENT 1 REPAIRS	B-07	BR-133-274
BENT 2 REPAIRS	B-08	BR-133-275
BENT 3 REPAIRS	B-09	BR-133-276
JOINT REPLACEMENT AND EPOXY OVERLAY DETAILS	B-10	BR-133-277
CONCRETE REPAIR DETAILS	B-11	BR-133-278

LIST OF BRIDGE REFERENCE DRAWINGS

(TO BE PRINTED WITH PLANS)
K-68-63 THRU K-68-68, STD-1-1

STANDARD STRUCTURES DRAWINGS

DRAWING	REV. DATE	DESCRIPTION
STD-10-3	01/10/2024	STANDARD FLUME DETAILS

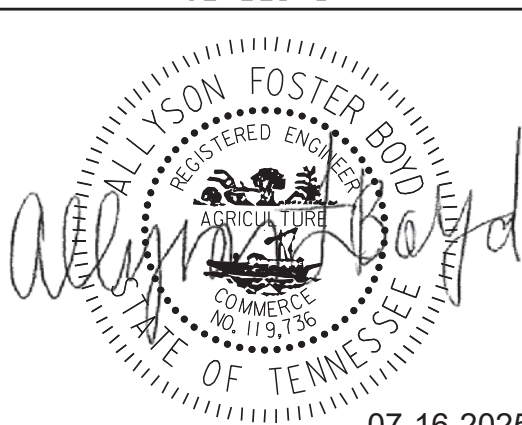
SPECIAL PROVISIONS

DRAWING	DESCRIPTION	REV. DATE
SP712PTQ	TRAFFIC QUEUE PROTECTION	10/07/2024

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	82I026-M3-003	1A
PS&E	2025	82I026-M3-003	1A

SULLIVAN COUNTYWESTFIELD DR.

SEALED BY




07-16-2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

INDEX
AND
STANDARD
DRAWINGS

PROJECT COMMITMENTS			
COMMITMENT ID	SOURCE DIVISON	DESCRIPTION	STA. / LOCATION
EDHZ001	ENVIRONMENTAL DIVISION, HAZARDOUS MATERIALS	An Asbestos Containing Material (ACM) survey was completed on Bridge No. 82I00260013 Westfield Drive over I-26 LM 0.05 (82-0B357-00.05). No asbestos was detected. Please see the report for further details and photographs. No special accommodations for demolition and waste disposal are anticipated for these structures and the material can be deposited in a C&D landfill. Prior to the demolition or rehabilitation of any structure (bridge or building), the contractor is required to submit the National Emission Standards for Hazardous Air Pollutants standard 10-day notice of demolition to the TDEC Division of Air Pollution Control (per TDOT Standard Specifications for Road and Bridge Construction (January 1, 2021) Sections 107.08.D and 202.03).	Bridge No. 82I00260013 Westfield Drive over I-26 LM 0.05 (82-0B357-00.05)

SEALED BY



07-16-2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PROJECT
COMMITMENTS

ESTIMATED ROADWAY QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
18	203-01 ROAD AND DRAINAGE EXCAVATION (UNCLASSIFIED)	C.Y.	1.8
	303-01 MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	3.64
	307-01.01 ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING A	TON	0.86
19	307-01.08 ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING B-IV2	TON	26
	402-01 BITUMINOUS MATERIAL FOR PRIME COAT	TON	1.00
	403-01 BITUMINOUS MATERIAL FOR TACK COAT	TON	1.00
	411-01.07 ASPHALT CONCRETE MIX (PG64-22) GRADING E SHOULDER	TON	0.33
	411-01.10 ACS MIX (PG64-22) GRADING D	TON	21
	415-01.02 COLD PLANING BITUMINOUS PAVEMENT	S.Y.	311
	620-05.10 CONCRETE PARAPET CURVED WALL (LESS THAN OR EQUAL TO 40 MPH)	LF.	24
	705-06.01 W-BEAM LOW VOLUME GUARDRAIL TRANSITION (TYPE 2) MASH TL-3	LF.	47
	705-06.10 GUARDRAIL TERMINAL TRAILING END (TYPE 13) MASH TL-3	EACH	1
	705-06.26 THRIE BEAM BRIDGE TRANSITION MASH TL-2	EACH	1
12	706-01 GUARDRAIL REMOVED	LF.	71
	709-05.06 MACHINED RIP-RAP (CLASS A-1)	TON	132
	1910712-01 TRAFFIC CONTROL	LS	1
	712-02.02 INTERCONNECTED PORTABLE BARRIER RAIL	LF.	500
	123712-02.10 PORTABLE BARRIER RAIL (MASH TL-3)	LF.	3,520
	414712-02.60 TEMPORARY WORK ZONE CRASH CUSHION (MASH TL-3)	EACH	4
	115712-04.01 FLEXIBLE DRUMS (CHANNELIZING)	EACH	310
	13712-04.50 BARRIER RAIL DELINEATOR	EACH	69
	564016712-06 SIGNS (CONSTRUCTION)	S.F.	1,424
	712-08.03 ARROW BOARD (TYPE C)	EACH	2
11	712-08.12 QUEUE PROTECTION TRUCK	DAY	10
	712-09.02 REMOVABLE PAVEMENT MARKING (8" BARRIER LINE)	LF.	26,520
	712-09.04 REMOVABLE PAVEMENT MARKING (STOP LINE)	LF.	70
	712-09.08 REMOVABLE PAVEMENT MARKING (6" LINE)	LF.	1,890
	712-09.31 REMOVABLE BLACK-OUT TAPE (8")	LF.	6,110
	713-16.01 CHANGEABLE MESSAGE SIGN UNIT	EACH	2
	716-12.02 ENHANCED FLATLINE THERMO PVMIT MRKNG (6IN LINE)	L.V.	0.3
	717-01 MOBILIZATION	LS	1
	730-40 TEMPORARY TRAFFIC SIGNAL SYSTEM	EACH	1
	20740-10.04 GEOTEXTILE (TYPE IV) (STABILIZATION)	S.Y.	173

FOOTNOTES

- ① ALL COSTS ASSOCIATED WITH INSTALLING, STORING, AND RE-INSTALLING ALL TRAFFIC CONTROL DEVICES DURING AND BETWEEN THE DIFFERENT TRAFFIC CONTROL PHASES WILL BE INCLUDED IN THE BID ITEM OF EACH ITEM. DURING THE TIME BETWEEN DIFFERENT TRAFFIC CONTROL PHASES, THE CONTRACTOR SHALL STORE ALL TRAFFIC CONTROL DEVICES IN A PROPER LOCATION THAT WILL NOT INTERFERE WITH THE TRAFFIC FLOW. ALL WORK MUST MEET THE FULL APPROVAL OF THE TDOT ENGINEER.
- ② ITEM SHALL INCLUDE INSTALLING AND RELOCATING 880 L.F. PORTABLE BARRIER RAIL FOR FOUR (4) SEPARATE WEEKENDS. THIS CONSTRUCTION PHASE IS TO BE CONDUCTED BETWEEN FRIDAY 8PM TO MONDAY 6AM. OUTSIDE OF THESE HOURS, PORTABLE BARRIER RAIL SHALL BE MOVED TO THE SHOULDER AREAS ON I-26 TO ALLOW ALL TRAVEL LANES TO BE OPEN FOR TRAFFIC.
- ③ ITEM MAY BE REPLACED WITH MASH TL-3 APPROVED BARRIER FROM QPL 45, IF UNAVAILABLE. THE PAY ITEM WILL INCLUDE FURNISHING AND INSTALLING ALL COMPONENTS AS LISTED FOR THE APPROVED BARRIER.
- ④ THIS ITEM SHALL BE A PORTABLE ENERGY ABSORBING TERMINAL MEETING THE REQUIREMENTS OF NCHRP 350 FOR TEST LEVEL 3. THE PAY ITEM WILL INCLUDE FURNISHING AND INSTALLING ALL COMPONENTS AS LISTED ON THE MANUFACTURER'S SHOP DRAWING.
- ⑤ THIS ITEM INCLUDES THE INSPECTION AND MAINTENANCE OF ANY SIGNING AND TRAFFIC CONTROL APPURTENANCES DURING THE CONSTRUCTION OPERATIONS.
- ⑥ INCLUDES ALL COSTS ASSOCIATED WITH THE INSTALLATION AND MAINTENANCE OF SIGN PANELS, SHEETING AND SUPPORTS.
- ⑦ TO SUPPORT LANE SHIFTS AND CHANGES TO TRAVEL PATH FOR THE LANE CLOSURES REQUIRED FOR CONSTRUCTION. ITEM INCLUDES APPLICATION AND REMOVAL OF TEMPORARY MARKNGS FOR EACH CONSTRUCTION PHASE.
- ⑧ ITEM CAN BE INCREASED OR DECREASED AS DIRECTED BY THE TDOT ENGINEER.
- ⑨ THIS ITEM INCLUDES ANY PREPARING (SMOOTHING) AND REPLACING OF RUMBLE STRIPS SHOULD THEY CONFLICT WITH THE WHEEL PATHS FOR TEMPORARY TRAFFIC.
- ⑩ SPEED LIMIT REDUCTION REQUIRES APPROVAL OF TDOT OPERATIONS PROJECT ENGINEER AND TDOT REGION 1 TRAFFIC ENGINEER.
- ⑪ QUEUES ARE EXPECTED TO EXCEED ONE (1) MILE ON I-26. TWO (2) TRUCKS WILL BE REQUIRED IN EACH DIRECTION, PLUS ONE (1) RESERVE TRUCK IN EACH DIRECTION. SEE SPECIAL PROVISION 712PTQ.
- ⑫ FOR FLUME. ITEM INCLUDES ALL MATERIALS AND LABOR NECESSARY FOR THE CONSTRUCTION.
- ⑬ INCLUDES 25 FOR WESTFIELD DRIVE AND 44 FOR I-26.
- ⑭ INCLUDES 2 FOR WESTFIELD DRIVE AND 2 FOR I-26.
- ⑮ INCLUDES 10 FOR WESTFIELD DRIVE AND 293 FOR I-26.
- ⑯ INCLUDES 385.5 SF FOR WESTFIELD DRIVE, 956 SF FOR I-26 AND 82.5 SF FOR VICTORY LANE DETOUR.
- ⑰ INCLUDES 1513 LF FOR WESTFIELD DRIVE AND 24998 LF FOR I-26.
- ⑱ FOR REMOVAL OF EXISTING SHOULDER AND RIP RAP AT RIGHT END BRIDGE.
- ⑲ INCLUDES 0.002 TON FOR SHOULDER REPAIR.
- ⑳ THIS ITEM SHALL INCLUDE ALL MATERIALS AND LABOR NECESSARY FOR CONSTRUCTION AND MAINTENANCE OF GEOTEXTILE REINFORCEMENT.

UTILITY OWNERS

WATER, SEWER:
CITY OF KINGSFORT
1213 KONNAROCK RD
KINGSFORT, TN 37664
CONTACT: CHAD AUSTIN
OFFICE PHONE: (423) 229-9454
CELL PHONE: (423) 224-2593
EMAIL: chadaustin@kingsporttn.gov

GAS:
SPECTRA ENERGY ENBRIDGE, FORMERLY EAST TENNESSEE NATURAL GAS
1575 DOWNTOWN WEST BLVD.
KNOXVILLE, TN 37919
CONTACT: GREG DAVISSON
OFFICE PHONE: (865) 539-3283
EMAIL: gbdavisson@spectraenergy.com

ELECTRIC:
KINGSFORT/ AMERICAN ELECTRIC/ APPALACHIAN POWER
420 RIVERPORT ROAD
CONTACT: TOM HENSLEY
OFFICE PHONE: (423) 578-2249
CELL PHONE: (423) 360-7497
EMAIL: tdhensley@aep.com

UTILITY NOTES

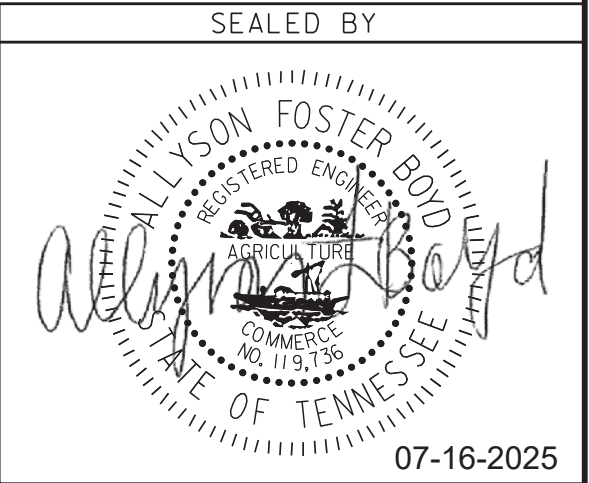
UTILITY

- (1) NO UTILITY CONFLICTS ARE ANTICIPATED BASED ON THE SCOPE OF WORK.

WESTFIELD DR TRAFFIC CONTROL SIGN QUANTITIES				
SIGN NO.	DESCRIPTION	SIZE	QUANTITY	AREA (S.F.)
G20-2	END ROAD WORK	48X24	3	24
M4-10L	DETOUR ARROW	48X18	1	6
R10-6	STOP HERE ON RED	24X36	3	18
R10-6 (MOD)	STAY IN LANE TO EXTEND GREEN	30X42	2	17.5
R11-2	ROAD CLOSED	48X30	1	10
W1-4L	LANE SHIFT	48X48	1	16
W16-2PC	1000 FEET	24X18	2	6
W20-1	ROAD WORK AHEAD	48X48	1	16
W20-1F	ROAD WORK 1500 FT	48X48	2	32
W20-1M	ROAD WORK 1/2 MILE	48X48	2	32
W20-4F	ONE LANE ROAD 1500 FT	48X48	2	32
W20-7	FLAGGER AHEAD	48x48	2	32
W20-F	ROAD WORK 1000 FT	48X48	2	32
W2-1F	ROAD WORK 500 FT	48X48	2	32
W3-3	SIGNAL AHEAD	48X48	3	48
W3-4	BE PREPARED TO STOP	48X48	2	32
TOTAL				385.5

I-26 TRAFFIC CONTROL SIGN QUANTITIES				
SIGN NO.	DESCRIPTION	SIZE	QUANTITY	AREA (S.F.)
G20-2	END ROAD WORK	48X24	4	32
R1-2	YIELD	48X48	1	16
R2-1	SPEED LIMIT	24X30	4	20
R4-1 (MOD.)	DO NOT PASS IN RIGHT LANE	120X42	2	70
TN-44	WORKERS PRESENT REDUCE SPEED	78X60	4	130
W1-4L	REVERSE CURVE SYMBOL	48X48	8	128
W3-2	YIELD AHEAD	48X48	1	16
W3-5	REDUCED SPEED LIMIT AHEAD	48X48	4	64
W4-1R	MERGE RIGHT	48X48	1	16
W4-2R	LANE ENDS RIGHT	48X48	4	64
W20-1	ROAD WORK AHEAD	48X48	7	112
W20-1M	ROAD WORK 2 MILES	48X48	4	64
W20-5RM	RIGHT LANE CLOSED 1 MILE	48X48	4	64
W20-5RM	RIGHT LANE CLOSED 1/2 MILE	48X48	4	64
W20-5RF	RIGHT LANE CLOSED 1500 FT	48x48	4	64
CUSTOM	MERGE NOW	48x48	2	32
TOTAL				956

DETOUR SIGN QUANTITIES				
SIGN NO.	DESCRIPTION	SIZE	QUANTITY	AREA (S.F.)
W20-3	ROAD CLOSED AHEAD	36X36	1	9
W20-2	DETOUR AHEAD	36X36	1	9
M4-9L	DETOUR LEFT	30X24	1	5
M4-9R	DETOUR RIGHT	30X24	3	15
M4-9T	DETOUR THROUGH	30X24	2	10
M4-8A	END DETOUR	24X18	1	3
CUSTOM	VICTORY LN	42X12	9	31.5
TOTAL				82.5



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED
ROADWAY
QUANTITIES &
UTILITY OWNERS

GENERAL NOTES

FINAL PAVEMENT MARKING

- (1) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

DETOURS, LANE SHIFTS AND MEDIAN CROSS-OVERS

- (17) BEFORE OPENING THE ROADWAY TO TRAFFIC, THE TRANSITIONAL MARKINGS ON THE EXISTING ROADWAY MUST BE IN PLACE. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 712-09.02 REMOVABLE PAVEMENT MARKING (8" BARRIER LINE) PER L.F. ALL EXISTING MARKINGS IN THE AREA OF THESE TRANSITIONAL MARKINGS SHALL BE OBLITERATED AND ALL EXISTING RAISED PAVEMENT MARKERS SHALL BE REMOVED TO ELIMINATE CONFLICTING MARKINGS. REMOVAL OF THE EXISTING CONFLICTING MARKINGS AND RAISED PAVEMENT MARKERS WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COST WILL BE INCLUDED IN ITEM NO. 712-01, TRAFFIC CONTROL, LUMP SUM.

TRAFFIC CONTROL DIRECTIONAL SIGNING

- (1) ON ALL ACCESS CONTROLLED AND INTERSTATE RECONSTRUCTION AND NEW CONSTRUCTION PROJECTS, THE CONTRACTOR SHALL UTILIZE ALL EXISTING DIRECTIONAL SIGNING FOR AS LONG AS POSSIBLE. THESE EXISTING SIGNS CAN BE MOVED USING TEMPORARY SUPPORTS AS NEEDED. AS SOON AS THESE EXISTING DIRECTIONAL SIGNS COME DOWN PERMANENTLY, THE CONTRACTOR SHALL HAVE UP AT LEAST ONE NEW TEMPORARY "ADVANCE GUIDE SIGN" AND ONE NEW TEMPORARY "EXIT DIRECTIONAL SIGN" AT ALL EXIT RAMPS. THESE SIGNS ARE TO BE MAINTAINED WITHIN CLEAR VIEW OF THE PUBLIC ON THE RIGHT SIDE OF THE HIGHWAY AND SHALL BE REPLACED IF DAMAGED, DURING ALL PHASES OF CONSTRUCTION, AS DIRECTED BY THE ENGINEER.
- (2) THE SIZE OF THESE NEW TEMPORARY SIGNS WILL BE DETERMINED BY THE MESSAGE. THE MESSAGE SHALL BE THE SAME AS THE EXISTING SIGN THAT THESE NEW TEMPORARY SIGNS WILL BE REPLACING. THE LETTER SIZE SHALL BE A MINIMUM OF 8 INCH, "D" UPPER CASE LETTER. THE DIRECTIONAL ARROW WILL BE A "B" ARROW AT A 45 DEGREE ANGLE (SAME ANGLE AS THE EXISTING ARROW). THE MATERIAL SHALL BE 0.100 INCH SHEET ALUMINUM; THE COLOR SHALL BE A REFLECTIVE GREEN BACKGROUND WITH REFLECTIVE WHITE COPY.
- (3) ALL WORK AND MATERIAL TO MAKE THESE NEW TEMPORARY DIRECTIONAL SIGNS ALONG WITH ADEQUATE SUPPORTS AND TO MOVE THEM AS NEEDED DURING EACH PHASE OF CONSTRUCTION WILL BE PAID FOR UNDER ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F. , AS DIRECTED BY THE ENGINEER.
- (4) SOME OF THESE DIRECTIONAL SIGNS WILL NEED AN INTERSTATE, U.S., OR A STATE HIGHWAY SHIELD, A CARDINAL DIRECTION, AND A DIRECTION ARROW TO ACCOMPANY THE DIRECTIONAL SIGN. THESE SIGNS SHALL BE MOUNTED BELOW THE DIRECTIONAL SIGN.
- (5) ALL EXISTING "EMERGENCY REFERENCE MARKERS" AND "HOSPITAL SIGNS" SHALL BE MAINTAINED WITHIN FULL VIEW OF THE MOTORING PUBLIC THROUGHOUT ALL PHASES OF CONSTRUCTION. ALL WORK IN MOVING AND TEMPORARY SUPPORTS SHALL BE PAID FOR UNDER ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F. .

PAVEMENT

PAVING

- (1) THE CONTRACTOR SHALL BE REQUIRED TO PAVE IN THE DIRECTION OF TRAFFIC.
- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.
- (3) THE CONTRACTOR SHALL ATTACH A DEVICE TO THE SCREED OF THE PAVER SUCH THAT MATERIAL IS CONFINED AT THE END GATE AND EXTRUDES THE ASPHALT MATERIAL IN SUCH A WAY THAT RESULTS IN A

CONSOLIDATED WEDGE-SHAPE PAVEMENT EDGE OF APPROXIMATELY 25 TO 30 DEGREES AS IT LEAVES THE PAVER (MEASURED FROM A LINE PARALLEL TO THE PAVEMENT SURFACE.) THE DEVICE SHALL MEET THE REQUIREMENTS THAT ARE CURRENTLY SET FORTH IN SPECIAL PROVISION 407SE.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (8) ALL DETOURS SHALL BE PAVED, STRIPED, SIGNED, AND FLEXIBLE DRUMS ARE TO BE IN PLACE BEFORE IT IS OPENED TO TRAFFIC.
- (9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

SPECIAL NOTES

DEMOLITION, REPAIR, OR REHABILITATION OF BRIDGES

- (1) THE CONTRACTOR SHALL VERIFY THAT AN ASBESTOS SURVEY HAS BEEN COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATIONS ACTIVITIES (NOT INCLUDING ASPHALT MILLING OR OVERLAY).
- (2) ASBESTOS-CONTAINING MATERIALS (ACM) ABATEMENT IS THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE COMPLETED PRIOR TO ANY DEMOLITION, REPAIR OR REHABILITATION OF BRIDGE(S). ABATEMENT SHOULD BE ACCOMPLISHED PER SP202ACM SPECIAL

PROVISION REGARDING REMOVAL OF ASBESTOS-CONTAINING MATERIALS. STATE OF TENNESSEE ASBESTOS ACCREDITATION REQUIREMENTS (TCA 1200-01-20) MANDATE THAT ACM ABATEMENT WORK BE PERFORMED BY AN ACCREDITED FIRM (CONTRACTOR) USING ACCREDITED ABATEMENT WORKERS AND SUPERVISORS.

- (3) THE CONTRACTOR SHALL BE RESPONSIBLE FOR SUBMITTING A NOTICE TO THE TDEC, DIVISION OF AIR POLLUTION CONTROL TEN (10) DAYS IN ADVANCE OF ANY ACM ABATEMENT, DEMOLITION, OR MAJOR REPAIR INVOLVING THE REMOVAL/REPLACEMENT OF A STRUCTURAL COMPONENT.

ENVIRONMENTAL GENERAL NOTES

PROJECT COMMITMENTS

- (1) SEE PROJECT COMMITMENTS, SHEET 1B FOR DETAILS RELATING TO SPECIAL ENVIRONMENTAL COMMITMENTS REQUIRED BY THIS PROJECT.

SCOPE OF WORK

- (2) THIS PROJECT IS A BRIDGE REPAIR PROJECT TO RESTORE THE WESTFIELD DRIVE BRIDGE OVER INTERSTATE 26 THAT IS IN NEED OF REPAIRS.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

SEDIMENT CONTROL

- (2) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (3) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

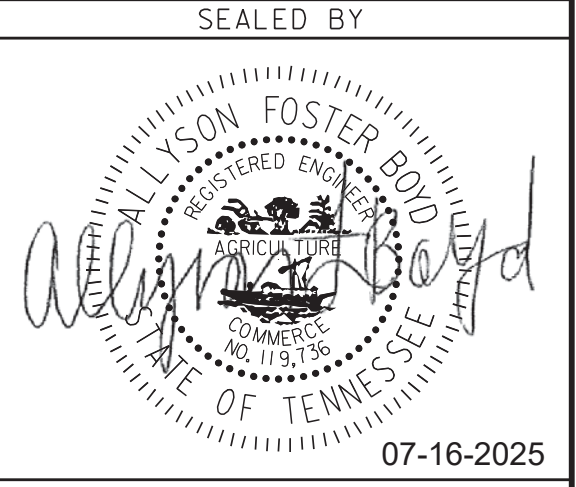
- (4) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (5) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (6) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.

- (7) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	2B
PS&E	2025	821026-M3-003	2B

SULLIVAN COUNTYWESTFIELD DR.

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DEPARTMENT OF TRANSPORTATION


GENERAL NOTES
AND
SPECIAL NOTES

- (8) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (9) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (10) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (11) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (12) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (13) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	2B1
PS&E	2025	821026-M3-003	2B1

SULLIVAN COUNTY WESTFIELD DR.

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

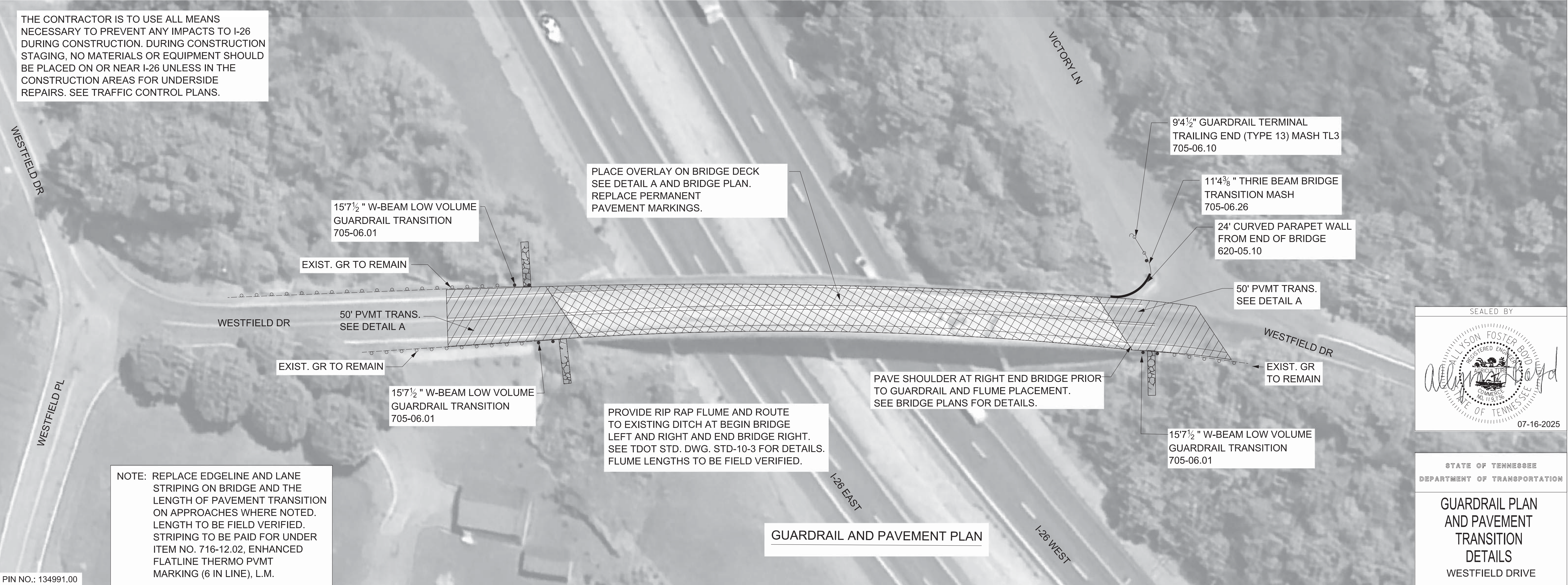
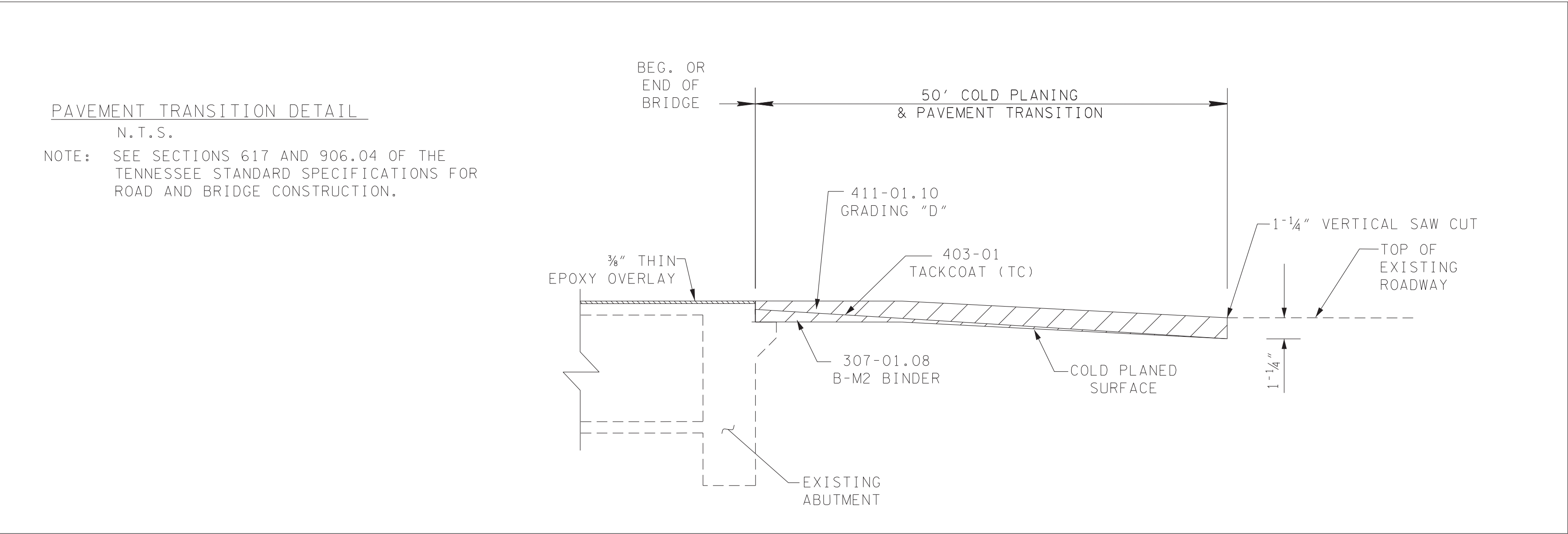
GENERAL NOTES
AND
SPECIAL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	2C
PS&E	2025	821026-M3-003	2C

SULLIVAN COUNTY

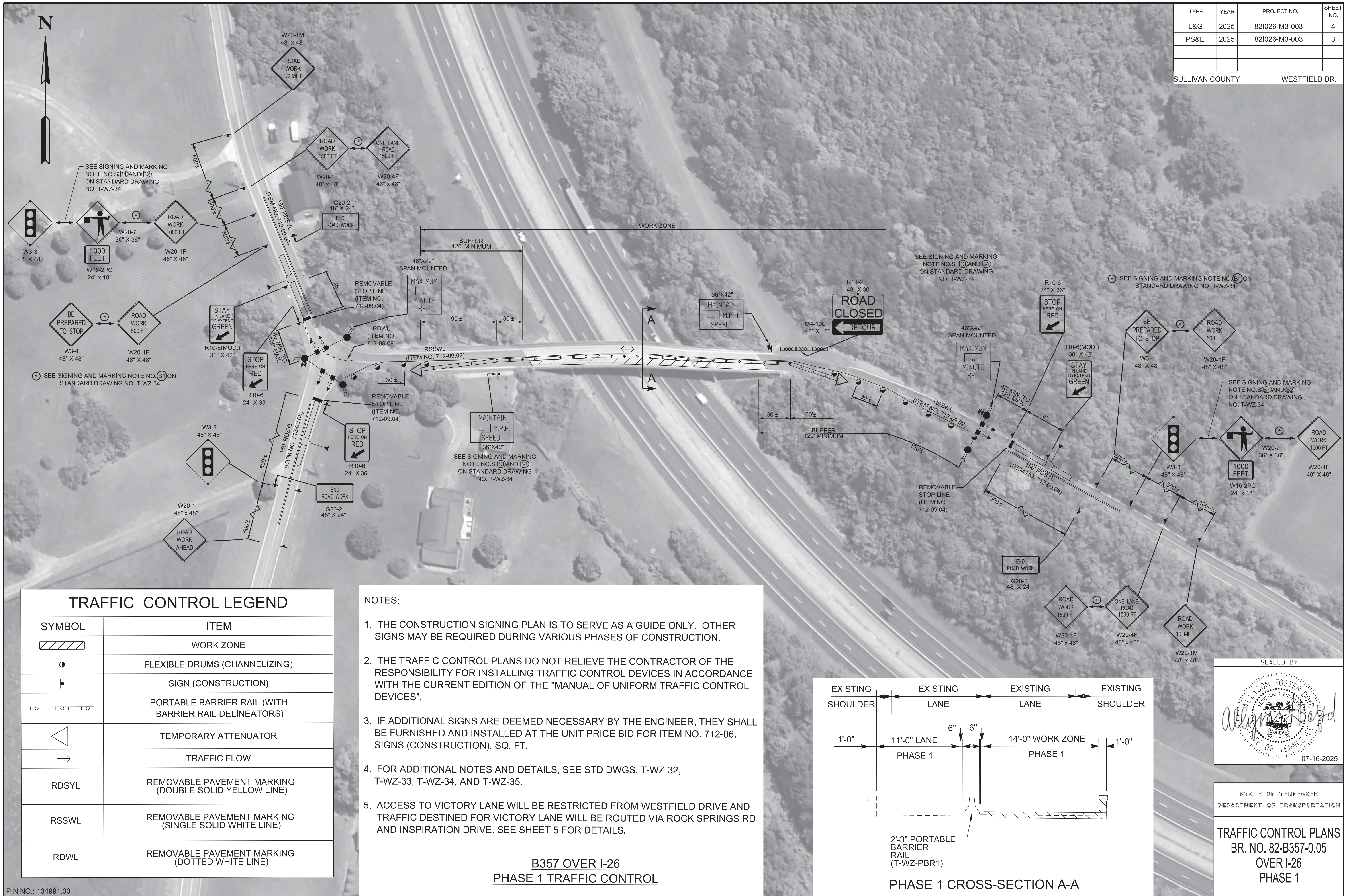
WESTFIELD DR.

DETAIL A

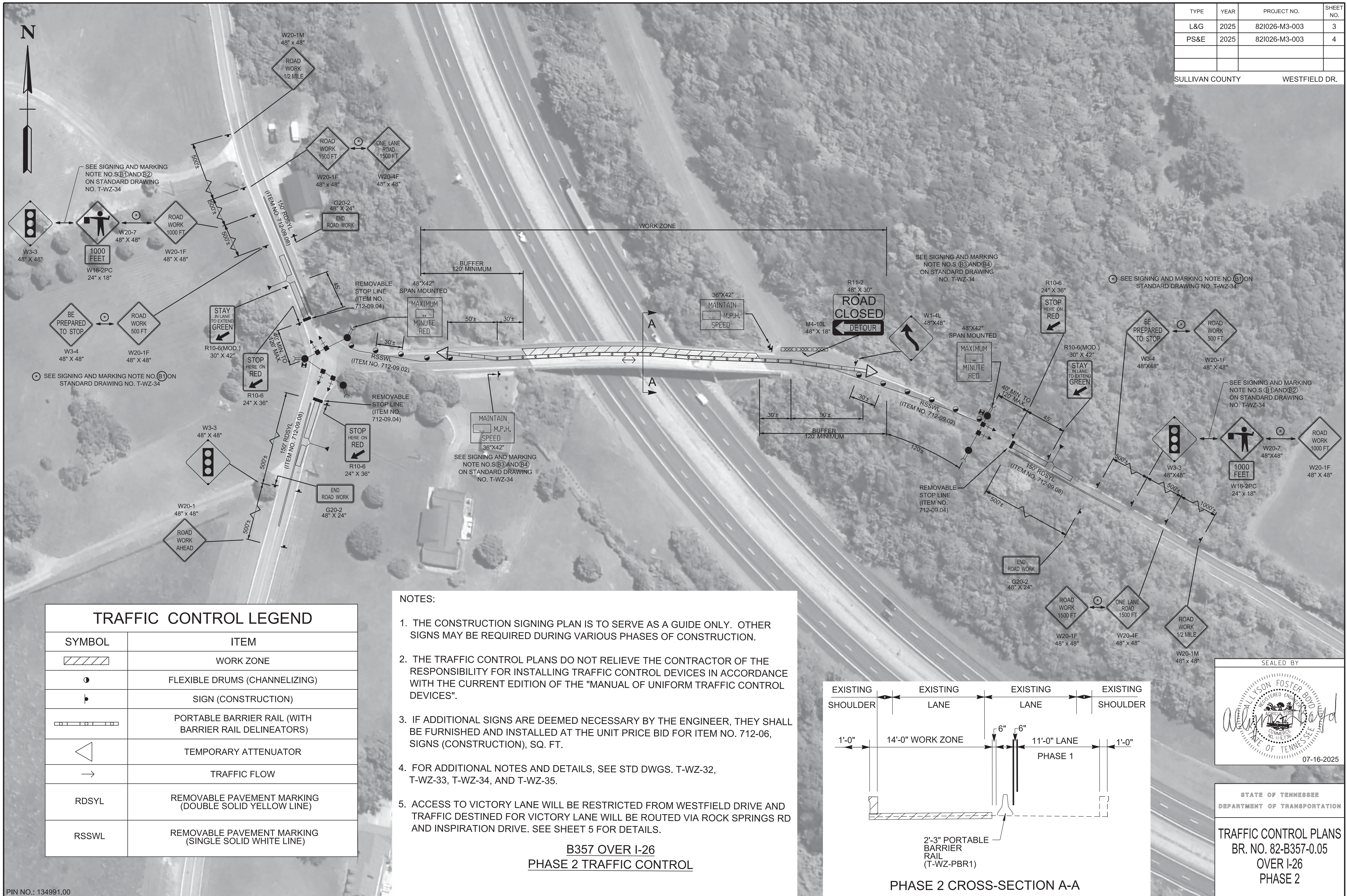


TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	82I026-M3-003	4
PS&E	2025	82I026-M3-003	3

SULLIVAN COUNTY
WESTFIELD DR.



TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	82I026-M3-003	3
PS&E	2025	82I026-M3-003	4
SULLIVAN COUNTY		WESTFIELD DR.	



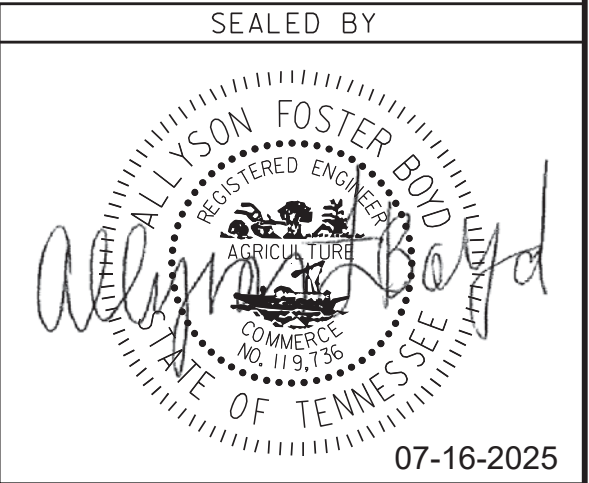


TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	3A
PS&E	2025	821026-M3-003	5
SULLIVAN COUNTY		WESTFIELD DR.	

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	SIGN (CONSTRUCTION)
	TEMPORARY BARRICADE (TYPE III)

NOTES:

- ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO CONSTRUCTION AND TO REMAIN IN PLACE UNTIL PROJECT COMPLETION. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
- THE TRAFFIC CONTROL PLANS DO NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY FOR INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES".
- THE LOCATION OF ALL TRAFFIC CONTROL DEVICES ARE TO BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION. TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
- SPECIAL DETOUR SIGNING SHALL BE BLACK COPY ON ORANGE BACKGROUND.
- SEE SHEETS 3, 4 & BR-133-270 FOR LANE RESTRICTIONS.



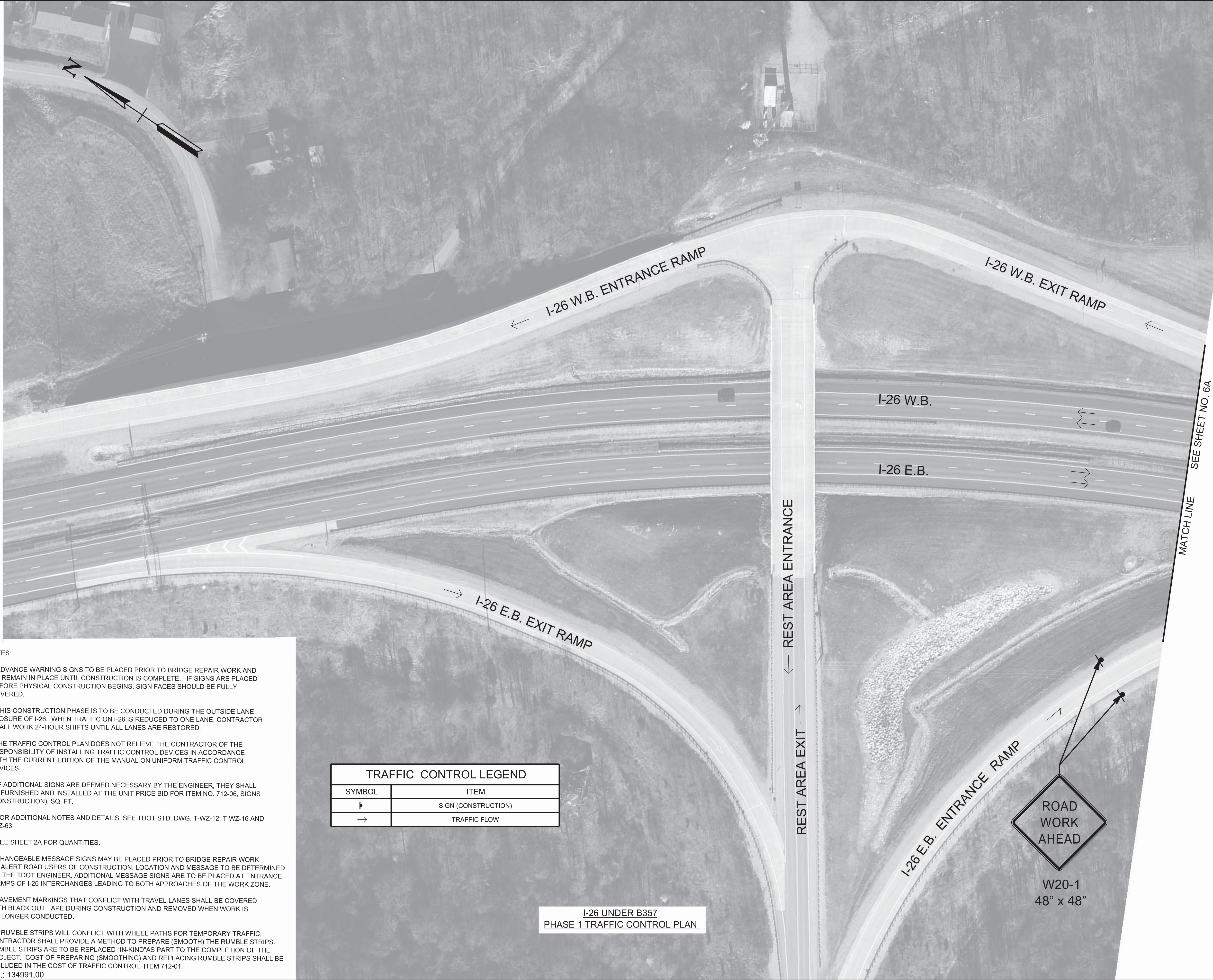
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
BR. NO. 82-B357-0.05
OVER I-26
DETOUR PLAN -
VICTORY LANE

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	5
PS&E	2025	821026-M3-003	6

SULLIVAN COUNTY

WESTFIELD DR.



NOTES:

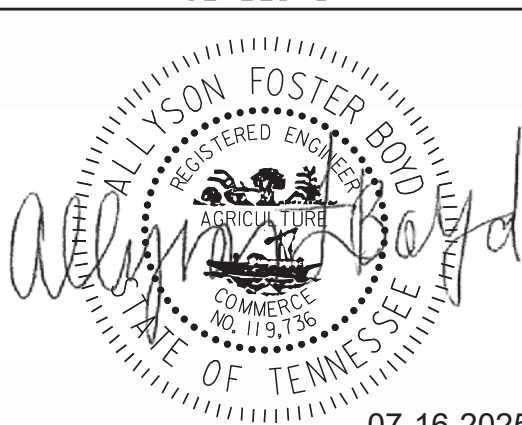
- ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
- THIS CONSTRUCTION PHASE IS TO BE CONDUCTED DURING THE OUTSIDE LANE CLOSURE OF I-26. WHEN TRAFFIC ON I-26 IS REDUCED TO ONE LANE, CONTRACTOR SHALL WORK 24-HOUR SHIFTS UNTIL ALL LANES ARE RESTORED.
- THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
- FOR ADDITIONAL NOTES AND DETAILS, SEE TDOT STD. DWG. T-WZ-12, T-WZ-16 AND T-WZ-63.
- SEE SHEET 2A FOR QUANTITIES.
- CHANGEABLE MESSAGE SIGNS MAY BE PLACED PRIOR TO BRIDGE REPAIR WORK TO ALERT ROAD USERS OF CONSTRUCTION. LOCATION AND MESSAGE TO BE DETERMINED BY THE TDOT ENGINEER. ADDITIONAL MESSAGE SIGNS ARE TO BE PLACED AT ENTRANCE RAMPS OF I-26 INTERCHANGES LEADING TO BOTH APPROACHES OF THE WORK ZONE.
- PAVEMENT MARKINGS THAT CONFLICT WITH TRAVEL LANES SHALL BE COVERED WITH BLACK OUT TAPE DURING CONSTRUCTION AND REMOVED WHEN WORK IS NO LONGER CONDUCTED.
- IF RUMBLE STRIPS WILL CONFLICT WITH WHEEL PATHS FOR TEMPORARY TRAFFIC, CONTRACTOR SHALL PROVIDE A METHOD TO PREPARE (SMOOTH) THE RUMBLE STRIPS. RUMBLE STRIPS ARE TO BE REPLACED "IN-KIND" AS PART TO THE COMPLETION OF THE PROJECT. COST OF PREPARING (SMOOTHING) AND REPLACING RUMBLE STRIPS SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL, ITEM 712-01.

PIN NO.: 134991.00

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
⬇	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW

I-26 UNDER B357
PHASE 1 TRAFFIC CONTROL PLAN

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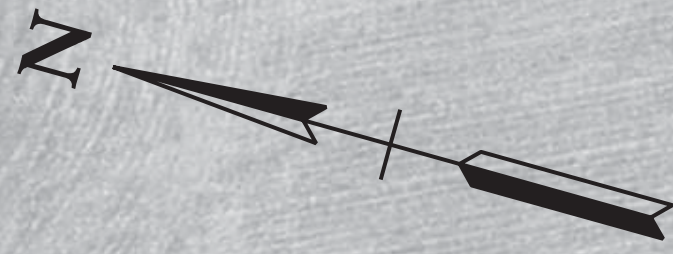
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 1

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	5A
PS&E	2025	821026-M3-003	6A

SULLIVAN COUNTY

WESTFIELD DR.



SEE SHEET NO. 6
MATCH LINE

I-26 W.B. EXIT RAMP

I-26 W.B.

I-26 E.B.

I-26 E.B. ENTRANCE RAMP

VARIABLE

SEE SHEET NO. 6B
MATCH LINE

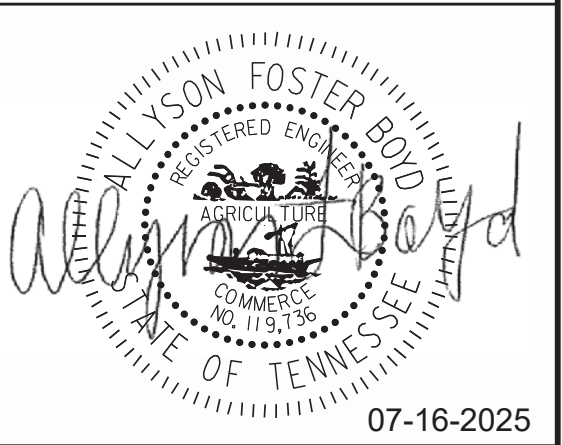
CHANGEABLE
MESSAGE SIGN
(SEE NOTE 7)

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW

I-26 UNDER B357
PHASE 1 TRAFFIC CONTROL PLAN

- NOTES:
- ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
 - THIS CONSTRUCTION PHASE IS TO BE CONDUCTED DURING THE OUTSIDE LANE CLOSURE OF I-26. WHEN TRAFFIC ON I-26 IS REDUCED TO ONE LANE, CONTRACTOR SHALL WORK 24-HOUR SHIFTS UNTIL ALL LANES ARE RESTORED.
 - THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 - IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
 - FOR ADDITIONAL NOTES AND DETAILS, SEE TDOT STD. DWG. T-WZ-12, T-WZ-16 AND T-WZ-63.
 - SEE SHEET 2A FOR QUANTITIES.
 - CHANGEABLE MESSAGE SIGNS MAY BE PLACED PRIOR TO BRIDGE REPAIR WORK TO ALERT ROAD USERS OF CONSTRUCTION. LOCATION AND MESSAGE TO BE DETERMINED BY THE TDOT ENGINEER. ADDITIONAL MESSAGE SIGNS ARE TO BE PLACED AT ENTRANCE RAMPS OF I-26 INTERCHANGES LEADING TO BOTH APPROACHES OF THE WORK ZONE.
 - PAVEMENT MARKINGS THAT CONFLICT WITH TRAVEL LANES SHALL BE COVERED WITH BLACK OUT TAPE DURING CONSTRUCTION AND REMOVED WHEN WORK IS NO LONGER CONDUCTED.
 - IF RUMBLE STRIPS WILL CONFLICT WITH WHEEL PATHS FOR TEMPORARY TRAFFIC, CONTRACTOR SHALL PROVIDE A METHOD TO PREPARE (SMOOTH) THE RUMBLE STRIPS. RUMBLE STRIPS ARE TO BE REPLACED "IN-KIND" AS PART TO THE COMPLETION OF THE PROJECT. COST OF PREPARING (SMOOTHING) AND REPLACING RUMBLE STRIPS SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL, ITEM 712-01.

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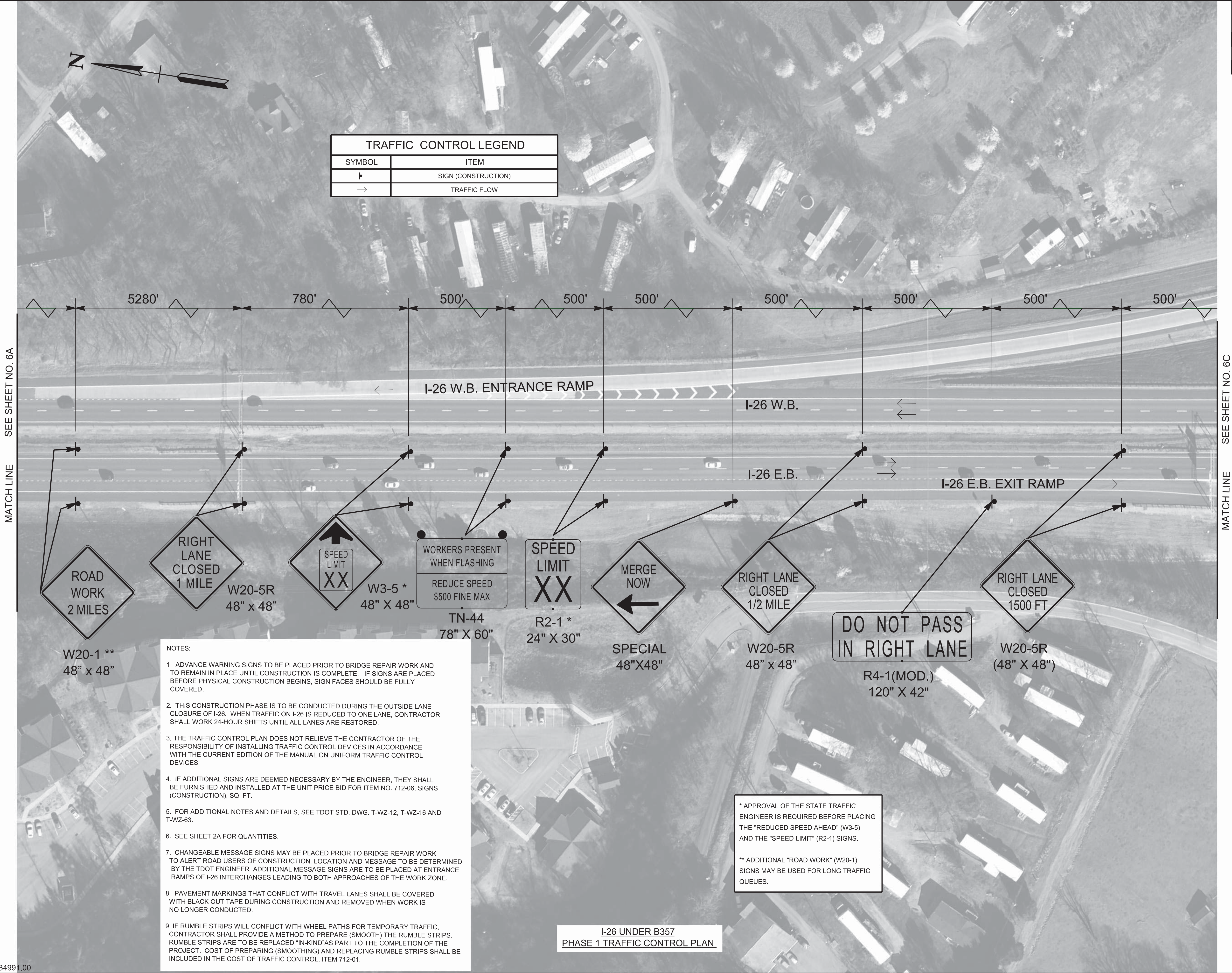


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 1

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	5B
PS&E	2025	821026-M3-003	6B

SULLIVAN COUNTYWESTFIELD DR.

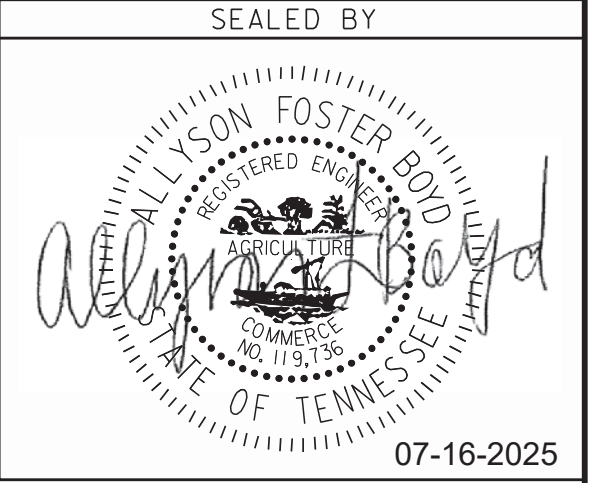


- NOTES:
1. ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
 2. THIS CONSTRUCTION PHASE IS TO BE CONDUCTED DURING THE OUTSIDE LANE CLOSURE OF I-26. WHEN TRAFFIC ON I-26 IS REDUCED TO ONE LANE, CONTRACTOR SHALL WORK 24-HOUR SHIFTS UNTIL ALL LANES ARE RESTORED.
 3. THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 4. IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
 5. FOR ADDITIONAL NOTES AND DETAILS, SEE TDOT STD. DWG. T-WZ-12, T-WZ-16 AND T-WZ-63.
 6. SEE SHEET 2A FOR QUANTITIES.
 7. CHANGEABLE MESSAGE SIGNS MAY BE PLACED PRIOR TO BRIDGE REPAIR WORK TO ALERT ROAD USERS OF CONSTRUCTION. LOCATION AND MESSAGE TO BE DETERMINED BY THE TDOT ENGINEER. ADDITIONAL MESSAGE SIGNS ARE TO BE PLACED AT ENTRANCE RAMPS OF I-26 INTERCHANGES LEADING TO BOTH APPROACHES OF THE WORK ZONE.
 8. PAVEMENT MARKINGS THAT CONFLICT WITH TRAVEL LANES SHALL BE COVERED WITH BLACK OUT TAPE DURING CONSTRUCTION AND REMOVED WHEN WORK IS NO LONGER CONDUCTED.
 9. IF RUMBLE STRIPS WILL CONFLICT WITH WHEEL PATHS FOR TEMPORARY TRAFFIC, CONTRACTOR SHALL PROVIDE A METHOD TO PREPARE (SMOOTH) THE RUMBLE STRIPS. RUMBLE STRIPS ARE TO BE REPLACED "IN-KIND" AS PART TO THE COMPLETION OF THE PROJECT. COST OF PREPARING (SMOOTHING) AND REPLACING RUMBLE STRIPS SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL, ITEM 712-01.

* APPROVAL OF THE STATE TRAFFIC ENGINEER IS REQUIRED BEFORE PLACING THE "REDUCED SPEED AHEAD" (W3-5) AND THE "SPEED LIMIT" (R2-1) SIGNS.

** ADDITIONAL "ROAD WORK" (W20-1) SIGNS MAY BE USED FOR LONG TRAFFIC QUEUES.

I-26 UNDER B357
PHASE 1 TRAFFIC CONTROL PLAN



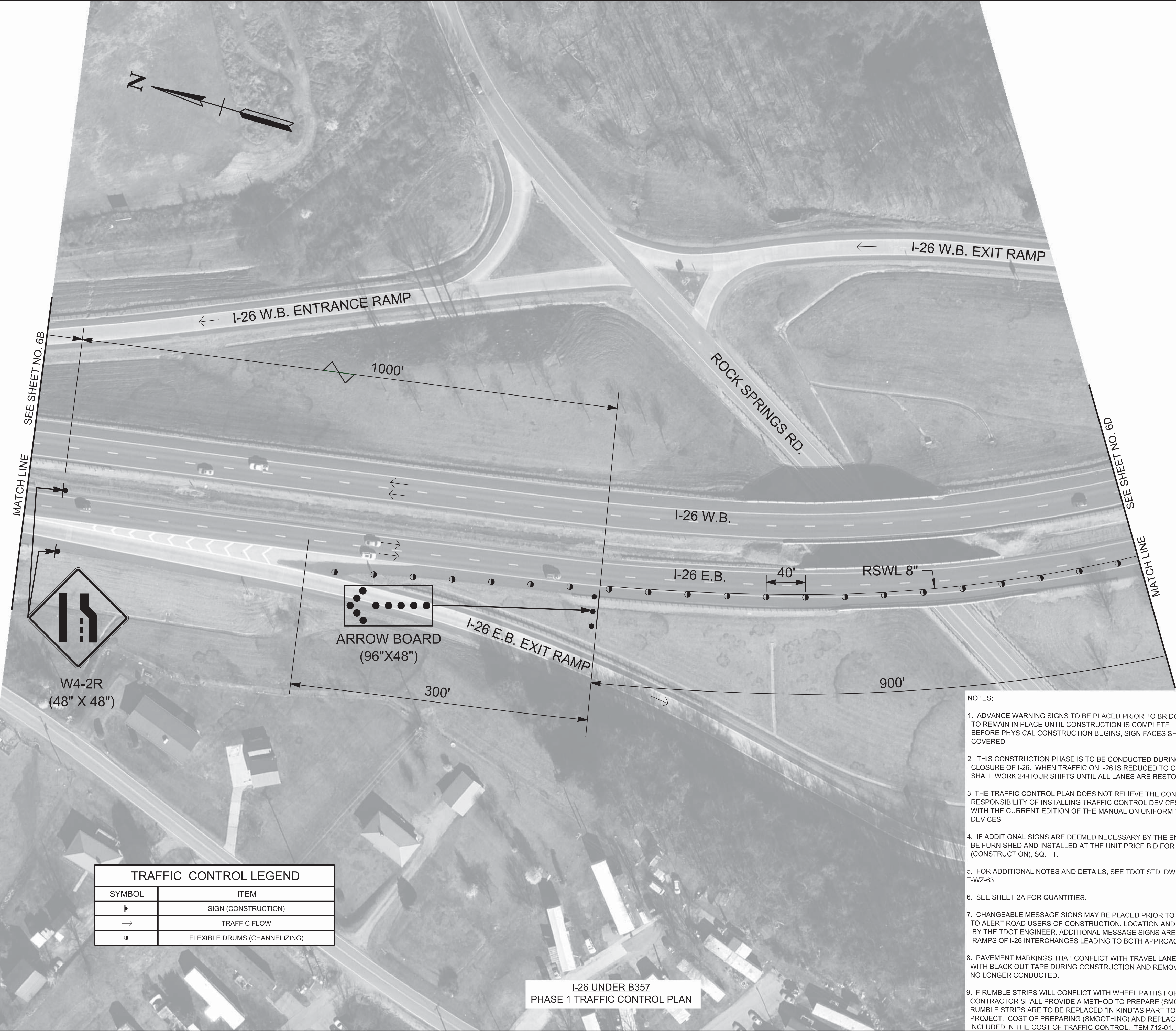
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 1

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	5C
PS&E	2025	821026-M3-003	6C


SULLIVAN COUNTY

WESTFIELD DR.



- NOTES:
- ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
 - THIS CONSTRUCTION PHASE IS TO BE CONDUCTED DURING THE OUTSIDE LANE CLOSURE OF I-26. WHEN TRAFFIC ON I-26 IS REDUCED TO ONE LANE, CONTRACTOR SHALL WORK 24-HOUR SHIFTS UNTIL ALL LANES ARE RESTORED.
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 - IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
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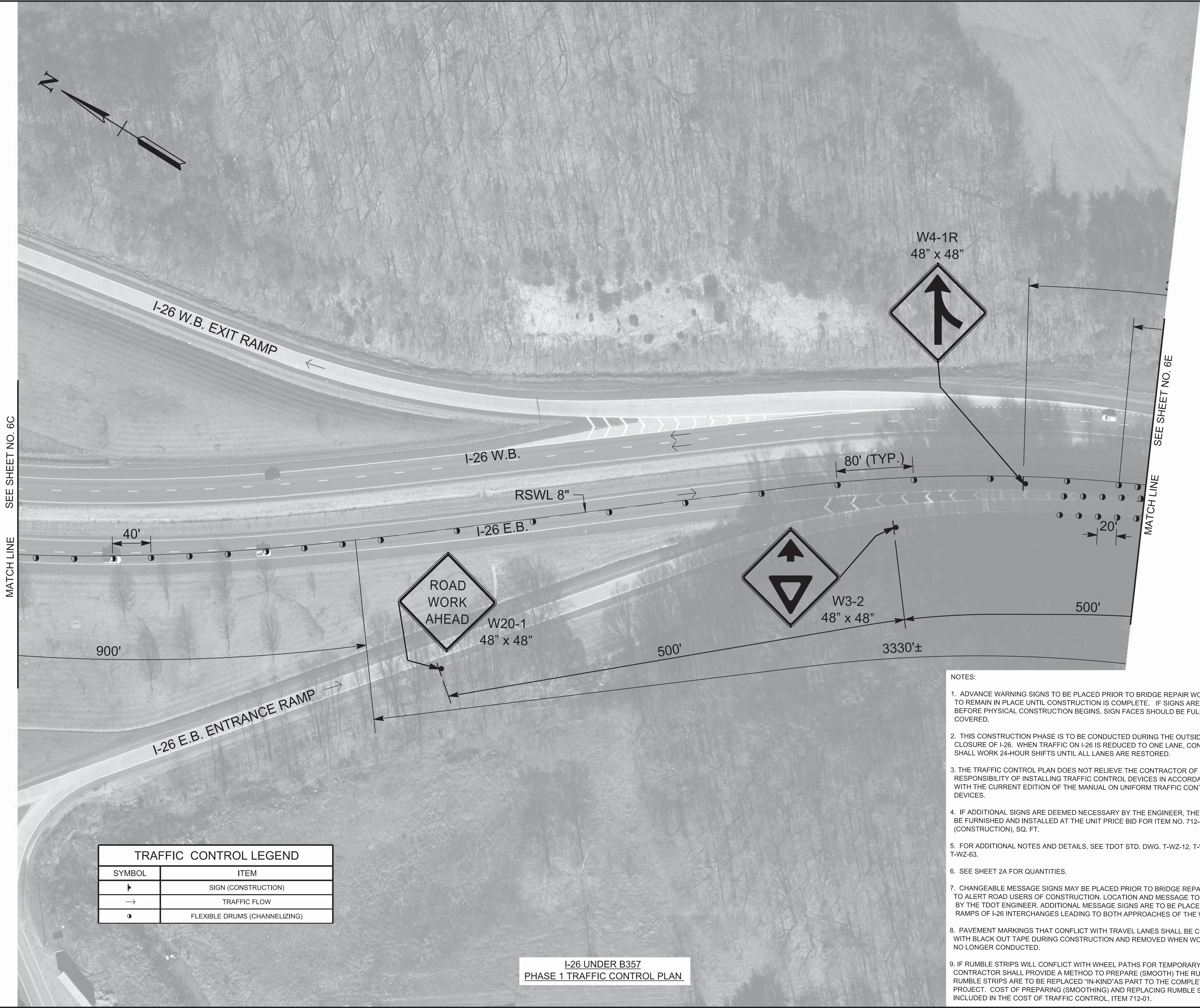
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 1

I-26 UNDER B357
PHASE 1 TRAFFIC CONTROL PLAN

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PIN NO.: 134991.00



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW
	FLEXIBLE DRUMS (CHANNELIZING)

I-26 UNDER B357
PHASE 1 TRAFFIC CONTROL PLAN

- NOTES:
- ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
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TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	5D
PS&E	2025	821026-M3-003	6D
SULLIVAN COUNTY		WESTFIELD DR.	

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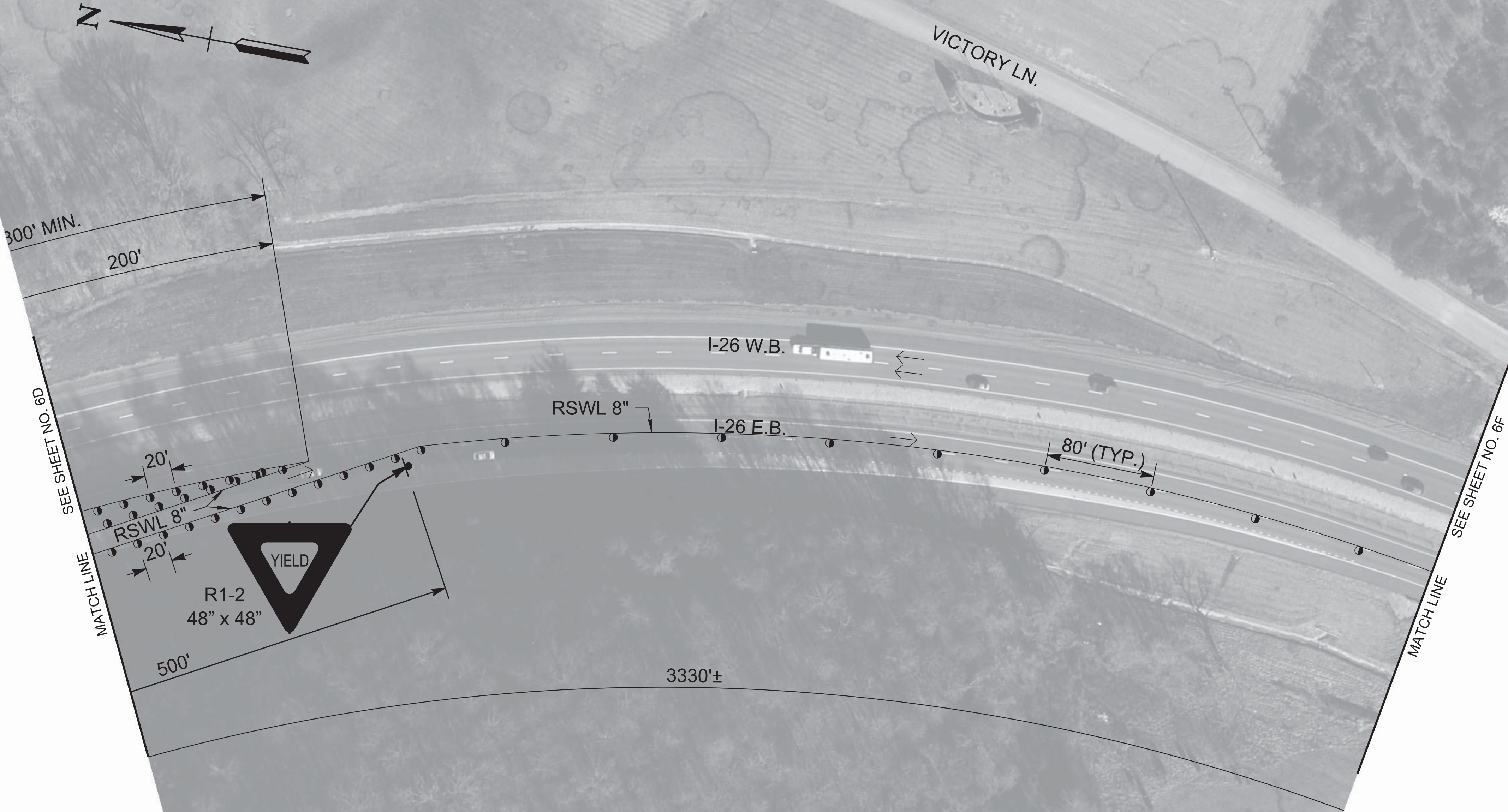
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 1

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	5E
PS&E	2025	821026-M3-003	6E

SULLIVAN COUNTY

WESTFIELD DR.



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
⬮	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW
●	FLEXIBLE DRUMS (CHANNELIZING)

I-26 UNDER B357
PHASE 1 TRAFFIC CONTROL PLAN

- NOTES:
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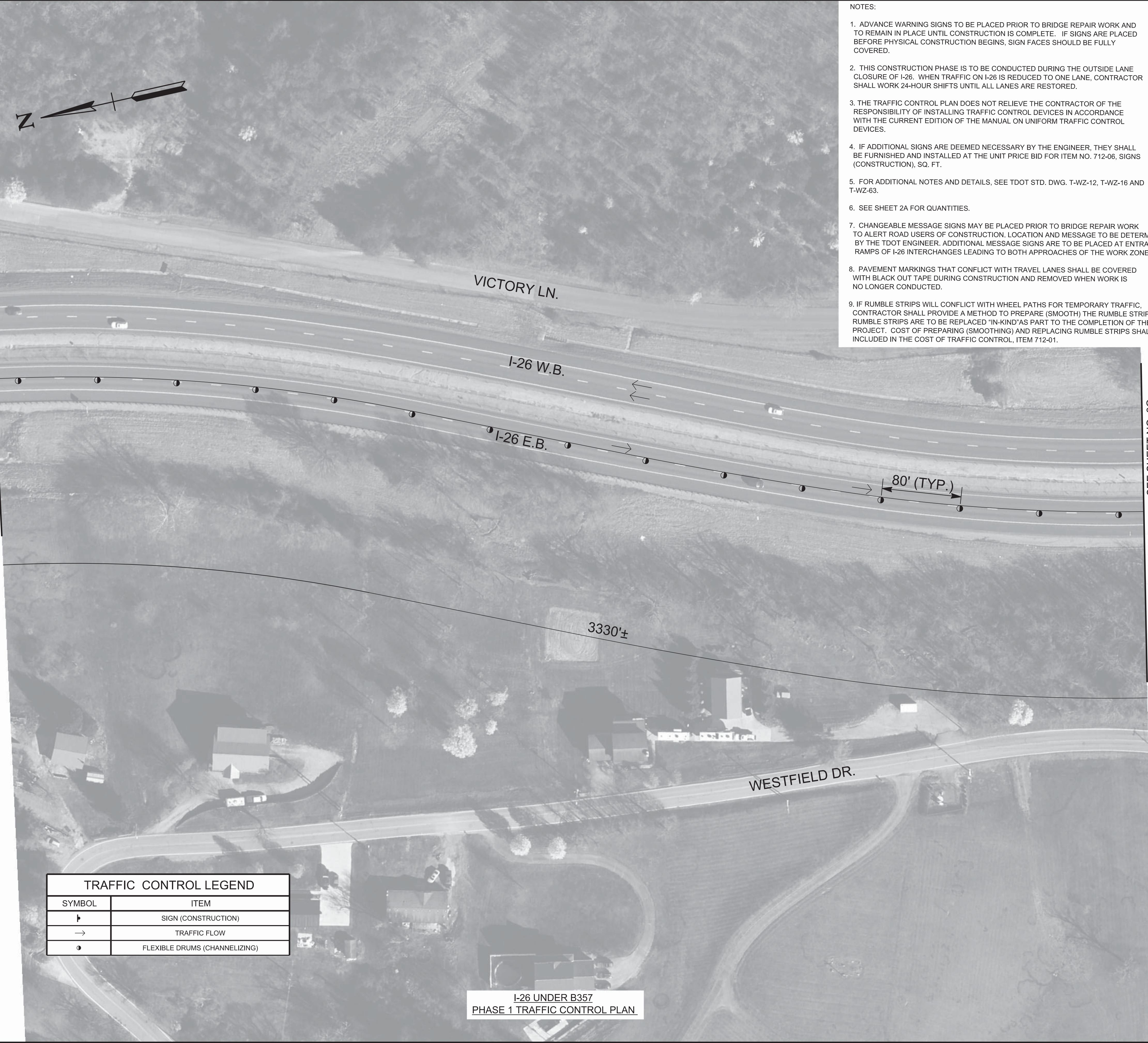
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ALLISON FOSTER BOYD
REGISTERED ENGINEER
AGRICULTURE
COMMERCIAL
NO. 118,736
STATE OF TENNESSEE
07-16-2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 1

MATCH LINE
SEE SHEET NO. 6E



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW
	FLEXIBLE DRUMS (CHANNELIZING)

I-26 UNDER B357
PHASE 1 TRAFFIC CONTROL PLAN

NOTES:

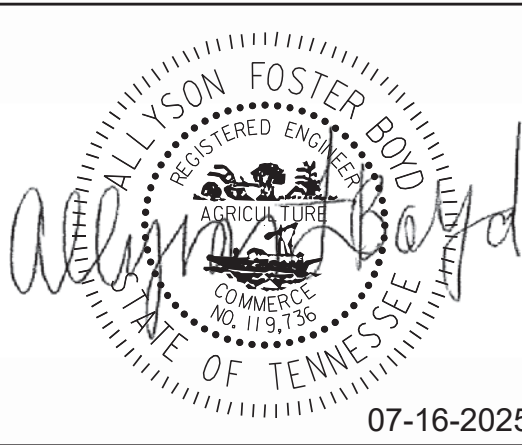
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TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	5F
PS&E	2025	821026-M3-003	6F

SULLIVAN COUNTY

WESTFIELD DR.

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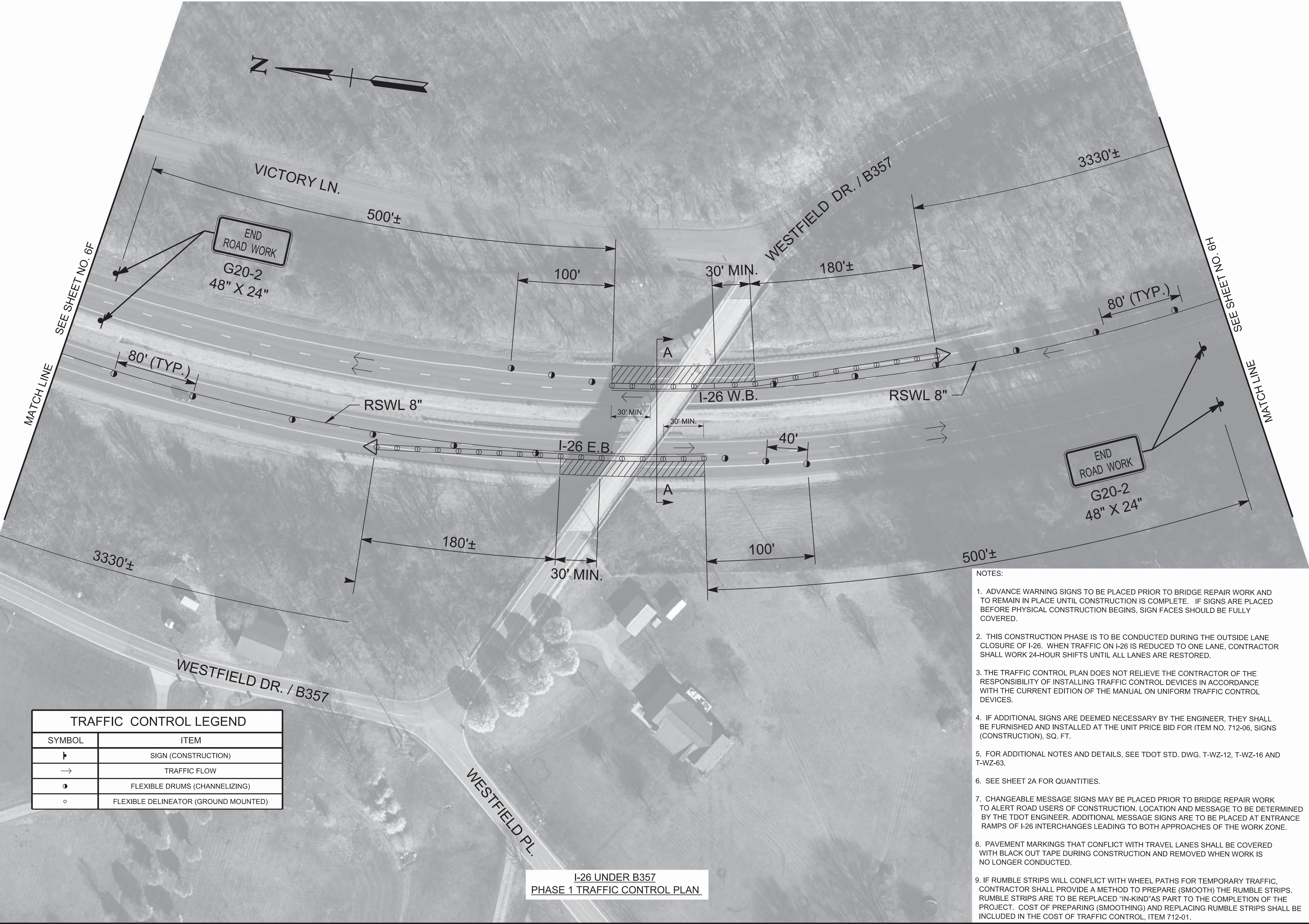
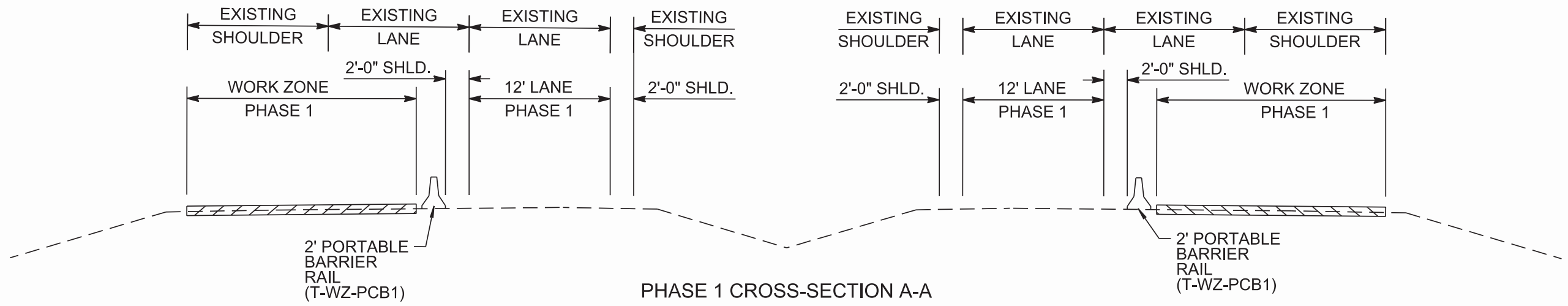


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 1

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	5G
PS&E	2025	821026-M3-003	6G

SULLIVAN COUNTY WESTFIELD DR.



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
└	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW
○	FLEXIBLE DRUMS (CHANNELIZING)
○	FLEXIBLE DELINEATOR (GROUND MOUNTED)

- NOTES:
1. ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
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SEALED BY

ALLISON FOSTER BOYD
REGISTERED ENGINEER
AGRICULTURE
COMMERCIAL
NO. 118,736
STATE OF TENNESSEE

07-16-2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

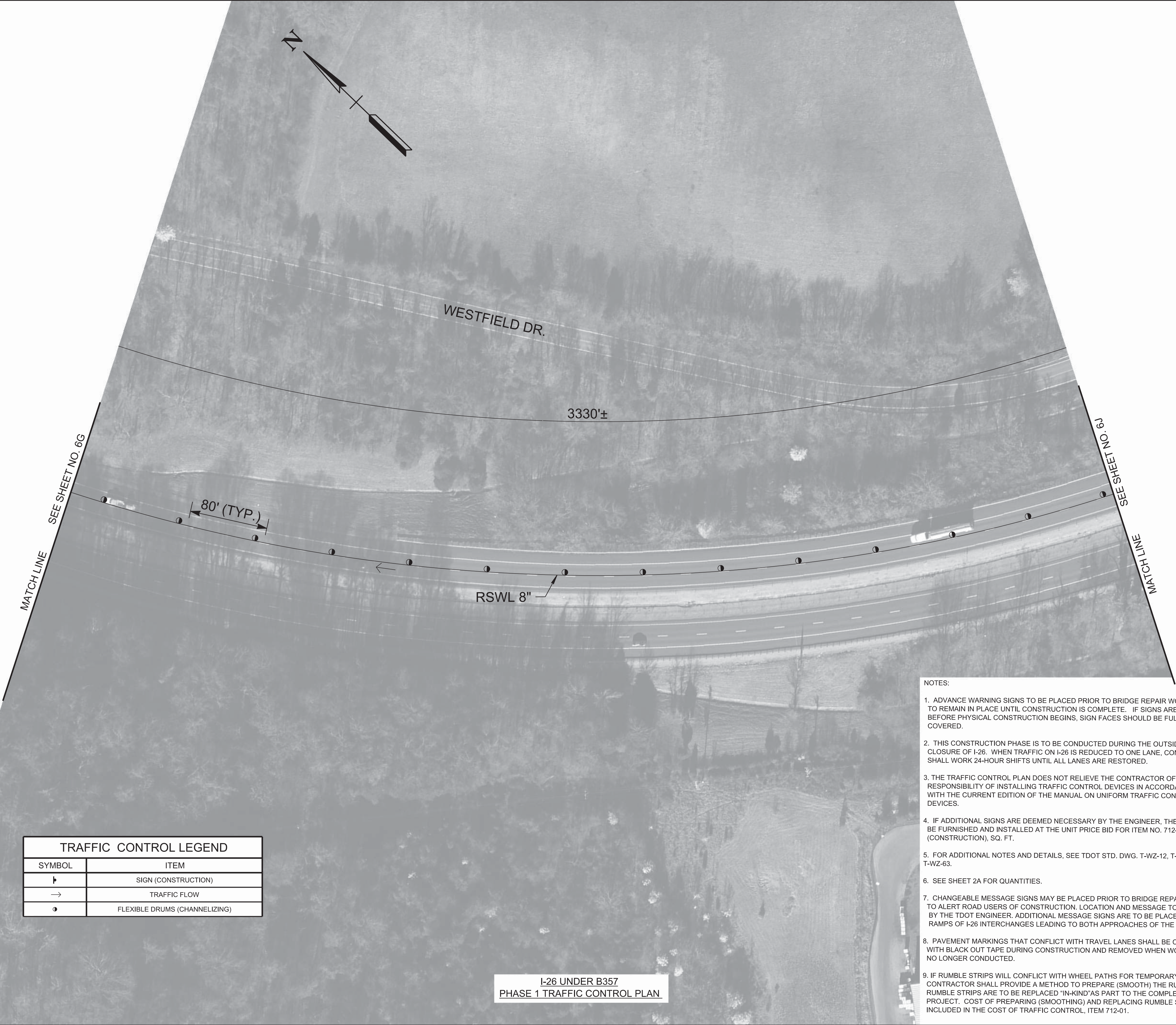
TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 1

I-26 UNDER B357
PHASE 1 TRAFFIC CONTROL PLAN

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	5H
PS&E	2025	821026-M3-003	6H

SULLIVAN COUNTY

WESTFIELD DR.



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
└	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW
●	FLEXIBLE DRUMS (CHANNELIZING)

- NOTES:
- ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
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I-26 UNDER B357
PHASE 1 TRAFFIC CONTROL PLAN

SEALED BY

ALLISON FOSTER BOYD
REGISTERED ENGINEER
AGRICULTURE
COMMERCIAL
NO. 118,736
STATE OF TENNESSEE
07-16-2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 1

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	5-I
PS&E	2025	821026-M3-003	6J

SULLIVAN COUNTY

WESTFIELD DR.



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
└	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW
●	FLEXIBLE DRUMS (CHANNELIZING)

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I-26 UNDER B357
PHASE 1 TRAFFIC CONTROL PLAN

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07-16-2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 1

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	5J
PS&E	2025	821026-M3-003	6K

SULLIVAN COUNTY

WESTFIELD DR.



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TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW
	FLEXIBLE DRUMS (CHANNELIZING)

I-26 UNDER B357
PHASE 1 TRAFFIC CONTROL PLAN

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07-16-2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 1

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	5K
PS&E	2025	821026-M3-003	6L
SULLIVAN COUNTY		WESTFIELD DR.	

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I-26 UNDER B357
PHASE 1 TRAFFIC CONTROL PLAN

SEALD BY

07-16-2025

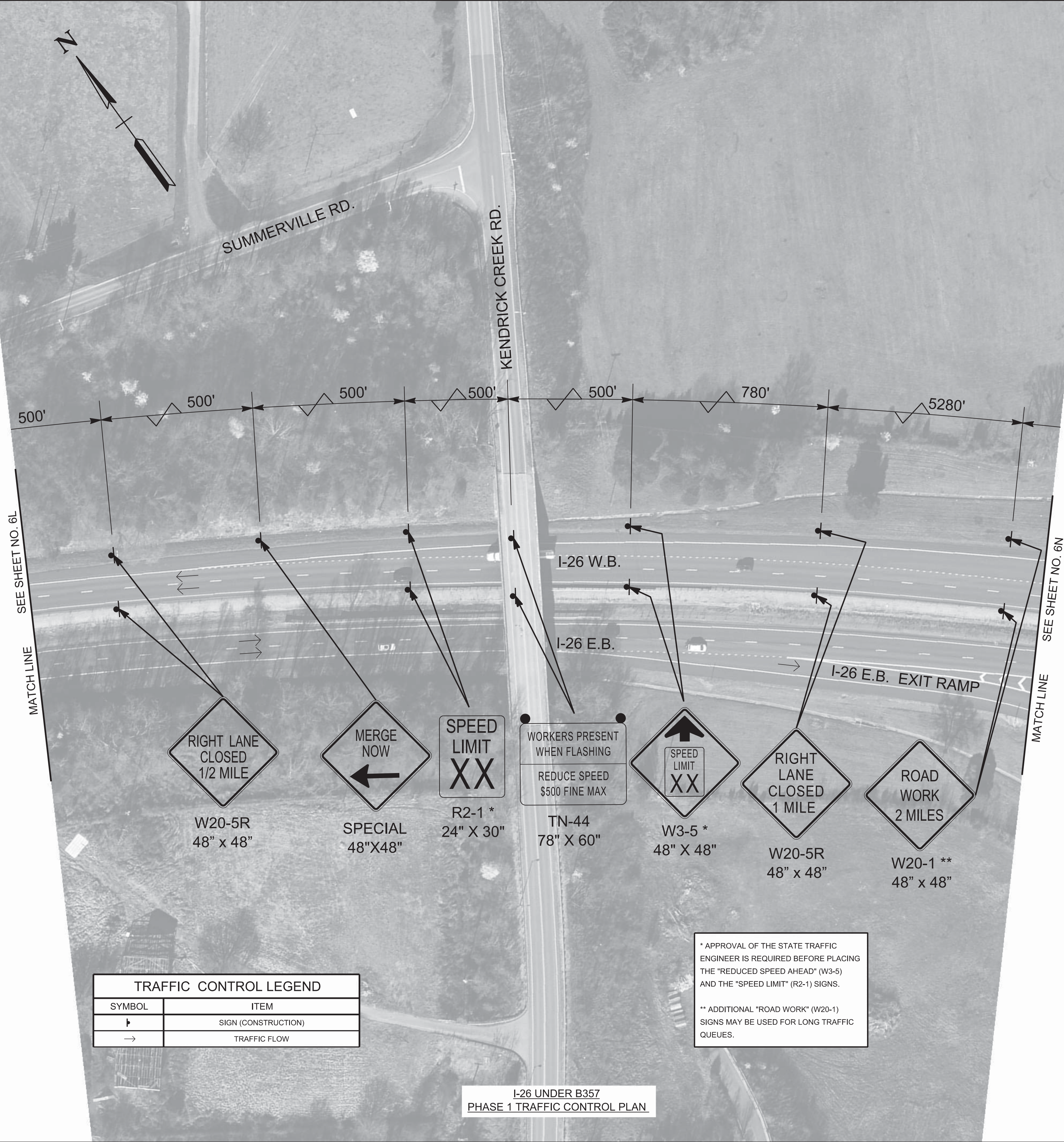
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 1

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	5L
PS&E	2025	821026-M3-003	6M

SULLIVAN COUNTY

WESTFIELD DR.



- NOTES:
- ADVANCE WARNING SIGNS TO BE PLACED PRIOR TO BRIDGE REPAIR WORK AND TO REMAIN IN PLACE UNTIL CONSTRUCTION IS COMPLETE. IF SIGNS ARE PLACED BEFORE PHYSICAL CONSTRUCTION BEGINS, SIGN FACES SHOULD BE FULLY COVERED.
 - THIS CONSTRUCTION PHASE IS TO BE CONDUCTED DURING THE OUTSIDE LANE CLOSURE OF I-26. WHEN TRAFFIC ON I-26 IS REDUCED TO ONE LANE, CONTRACTOR SHALL WORK 24-HOUR SHIFTS UNTIL ALL LANES ARE RESTORED.
 - THE TRAFFIC CONTROL PLAN DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF INSTALLING TRAFFIC CONTROL DEVICES IN ACCORDANCE WITH THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
 - IF ADDITIONAL SIGNS ARE DEEMED NECESSARY BY THE ENGINEER, THEY SHALL BE FURNISHED AND INSTALLED AT THE UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION), SQ. FT.
 - FOR ADDITIONAL NOTES AND DETAILS, SEE TDOT STD. DWG. T-WZ-12, T-WZ-16 AND T-WZ-63.
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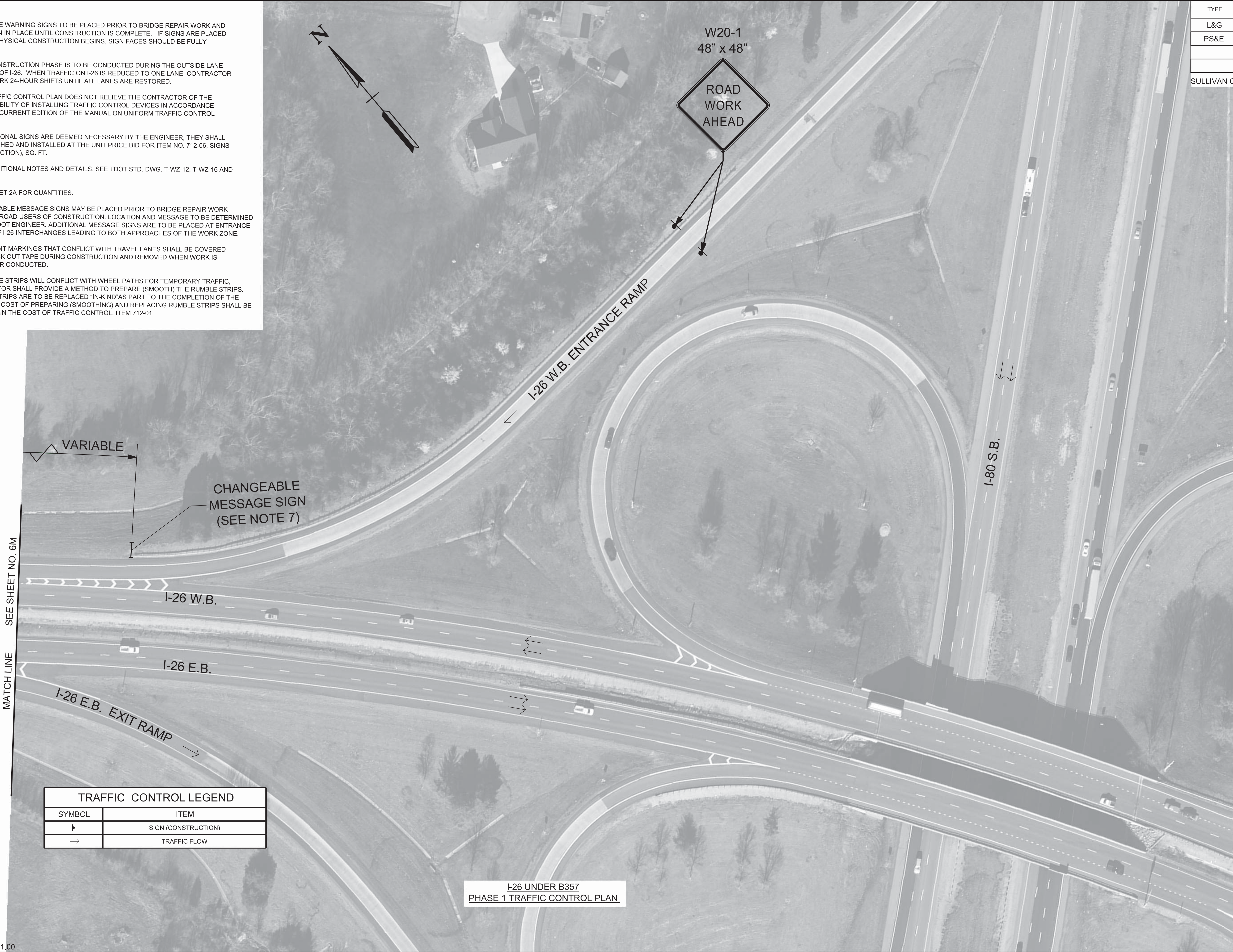
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 1

- NOTES:
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TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
→	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	5M
PS&E	2025	821026-M3-003	6N
SULLIVAN COUNTY		WESTFIELD DR.	

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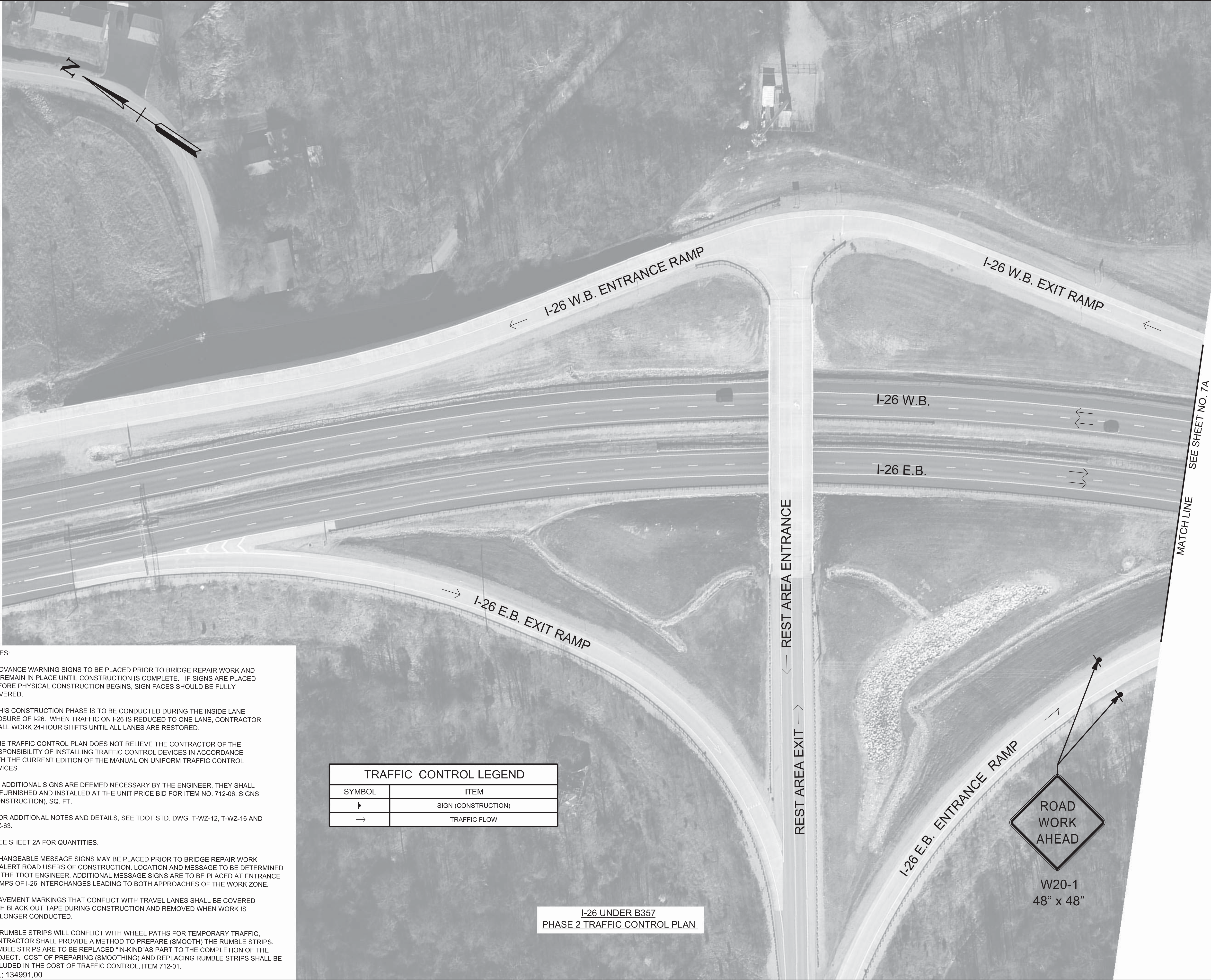
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 1

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	6
PS&E	2025	821026-M3-003	7

SULLIVAN COUNTY

WESTFIELD DR.



NOTES:

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PIN NO.: 134991.00

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
⬇	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW

I-26 UNDER B357
PHASE 2 TRAFFIC CONTROL PLAN

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07-16-2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 2

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	6A
PS&E	2025	821026-M3-003	7A

SULLIVAN COUNTY

WESTFIELD DR.



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW

I-26 UNDER B357
PHASE 2 TRAFFIC CONTROL PLAN.

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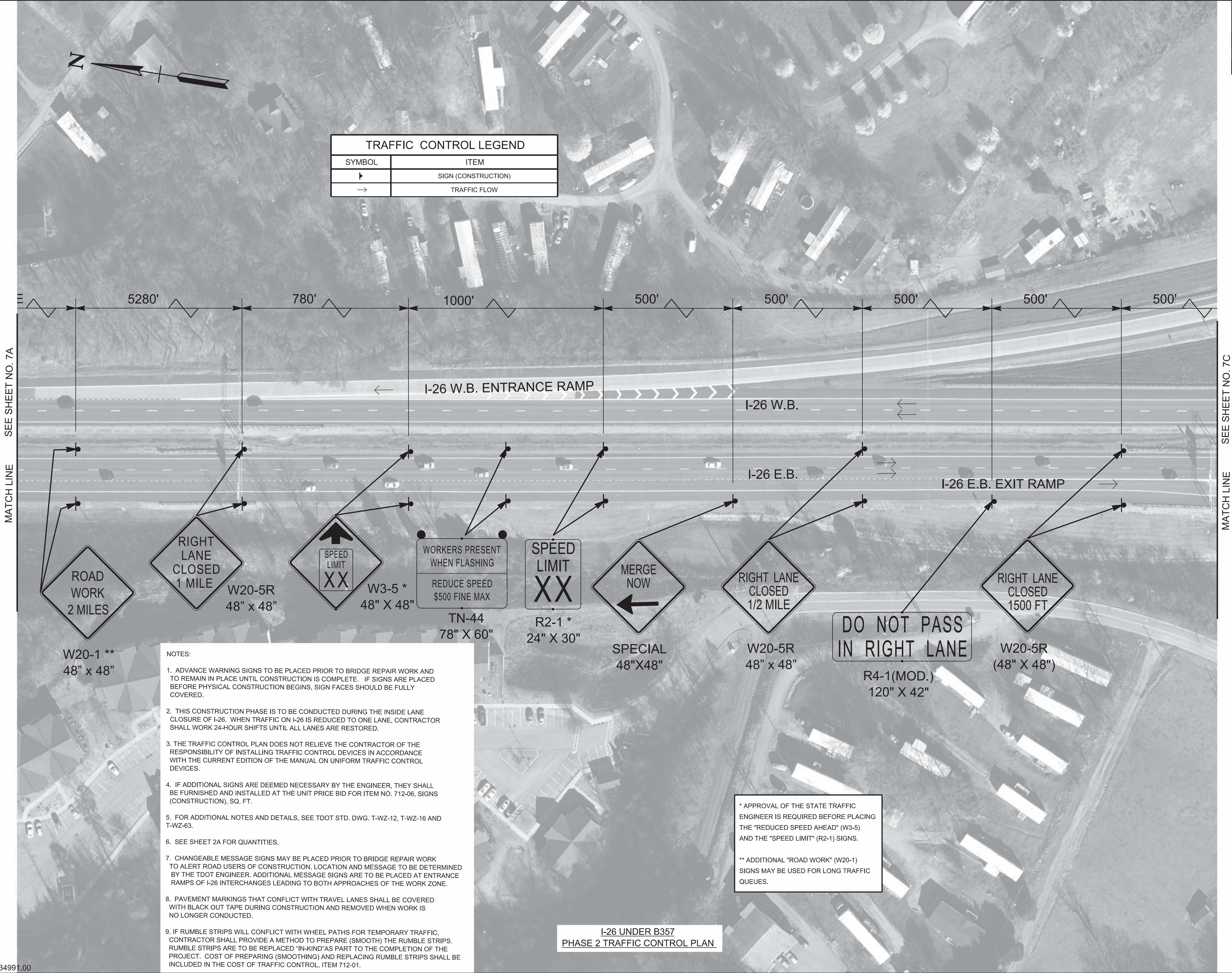
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 2

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	6B
PS&E	2025	821026-M3-003	7B

SULLIVAN COUNTYWESTFIELD DR.

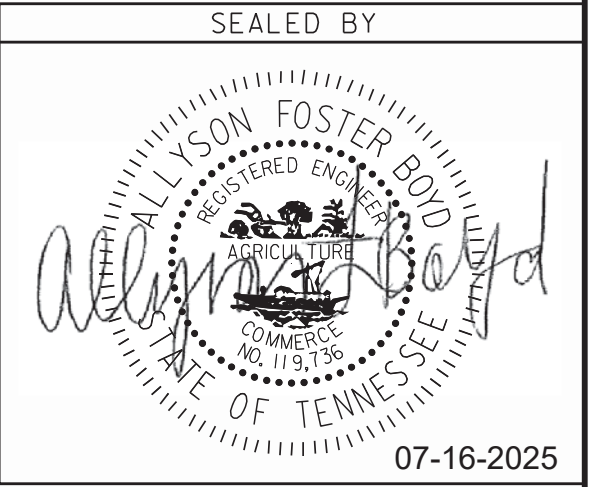


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* APPROVAL OF THE STATE TRAFFIC ENGINEER IS REQUIRED BEFORE PLACING THE "REDUCED SPEED AHEAD" (W3-5) AND THE "SPEED LIMIT" (R2-1) SIGNS.

** ADDITIONAL "ROAD WORK" (W20-1) SIGNS MAY BE USED FOR LONG TRAFFIC QUEUES.

I-26 UNDER B357
PHASE 2 TRAFFIC CONTROL PLAN



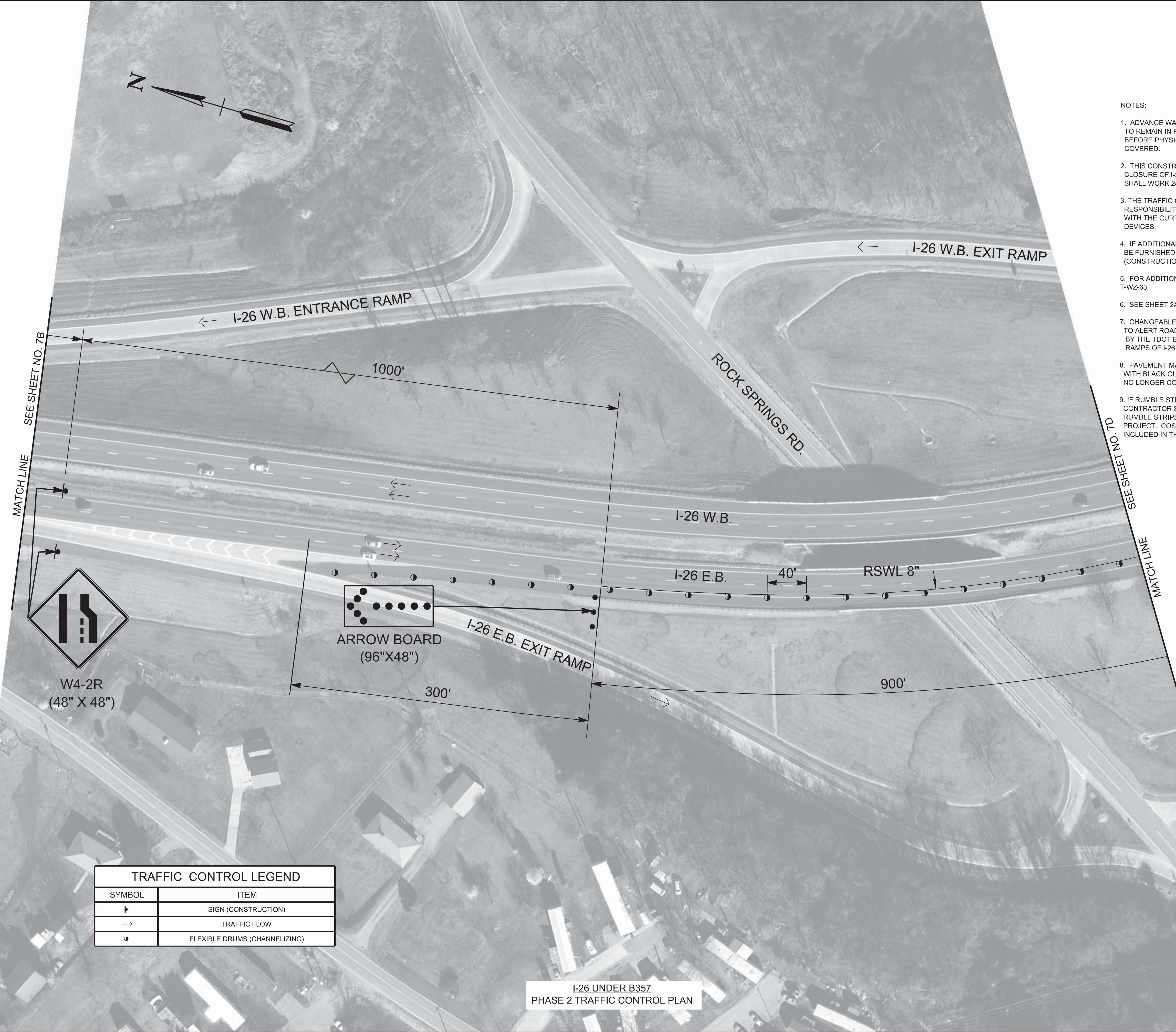
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 2

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	6C
PS&E	2025	821026-M3-003	7C

SULLIVAN COUNTY WESTFIELD DR.

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TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
⏏	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW
●	FLEXIBLE DRUMS (CHANNELIZING)

I-26 UNDER B357
PHASE 2 TRAFFIC CONTROL PLAN

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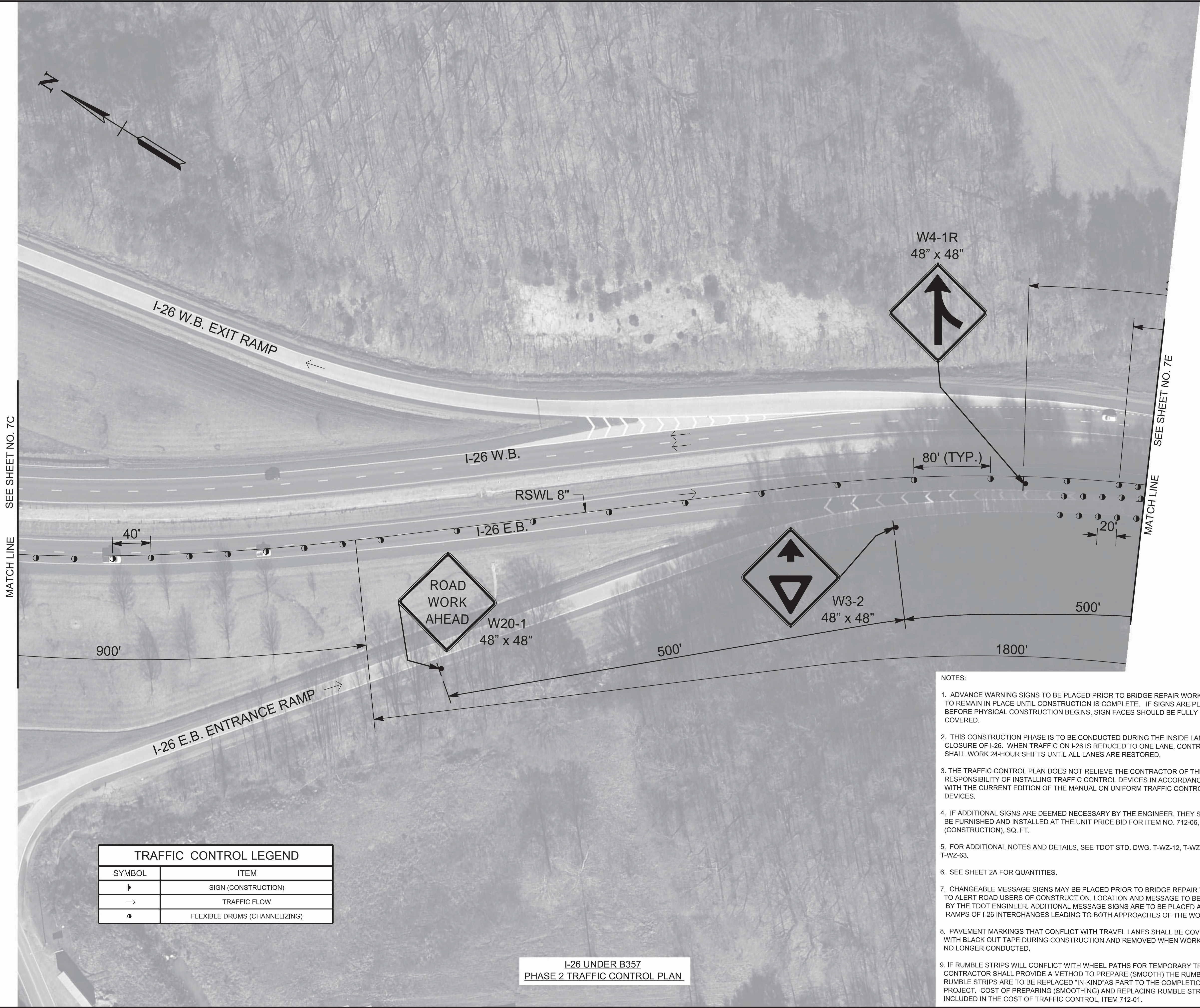
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 2

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TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW
	FLEXIBLE DRUMS (CHANNELIZING)


I-26 UNDER B357
PHASE 2 TRAFFIC CONTROL PLAN

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	6D
PS&E	2025	821026-M3-003	7D

SULLIVAN COUNTY WESTFIELD DR.

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07-16-2025

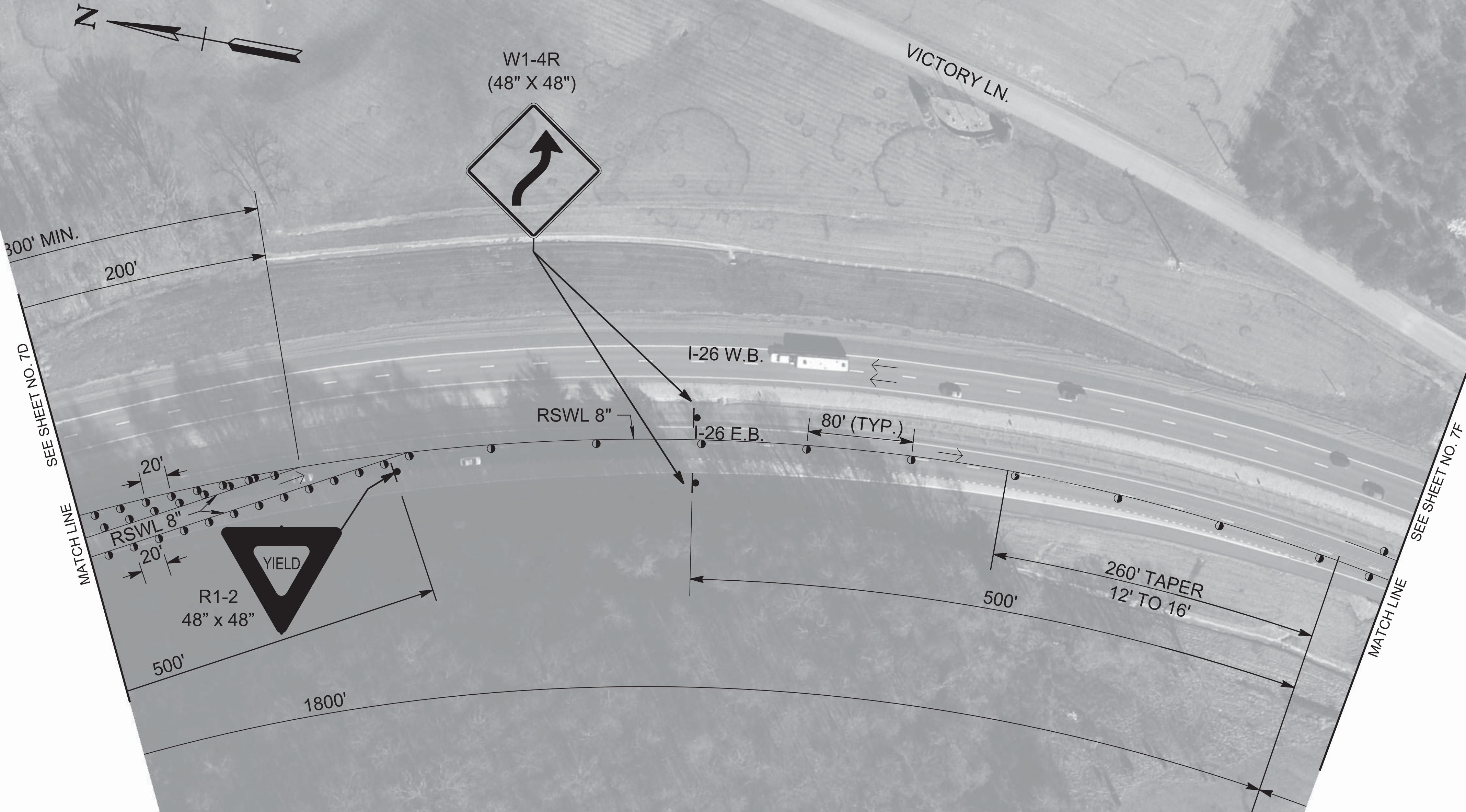
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 2

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	6E
PS&E	2025	821026-M3-003	7E

SULLIVAN COUNTY

WESTFIELD DR.



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
⌋	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW
●	FLEXIBLE DRUMS (CHANNELIZING)
////////	REMOVE PAVEMENT STRIPING

I-26 UNDER B357
PHASE 2 TRAFFIC CONTROL PLAN

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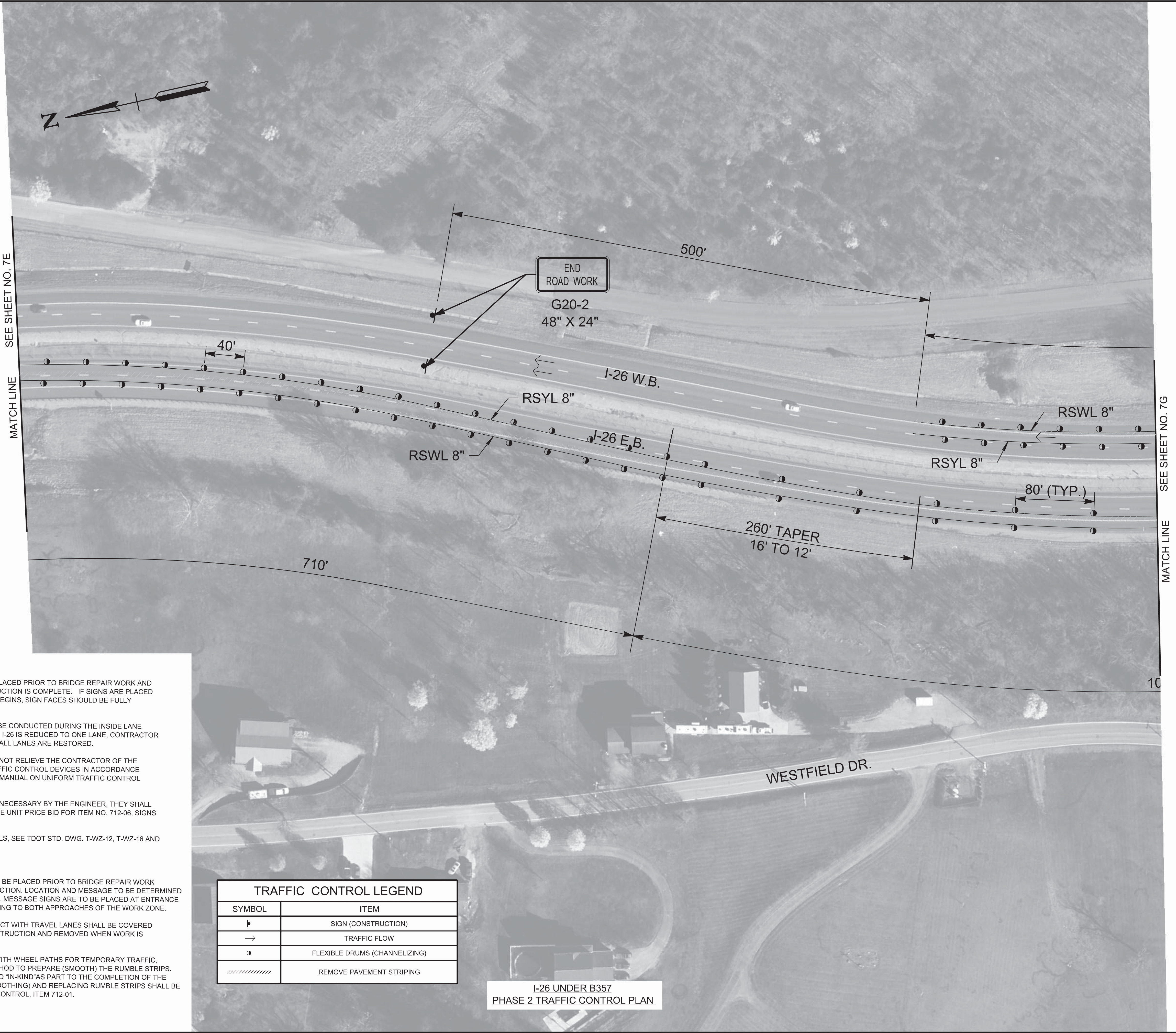
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 2

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	6F
PS&E	2025	821026-M3-003	7F

SULLIVAN COUNTYWESTFIELD DR.



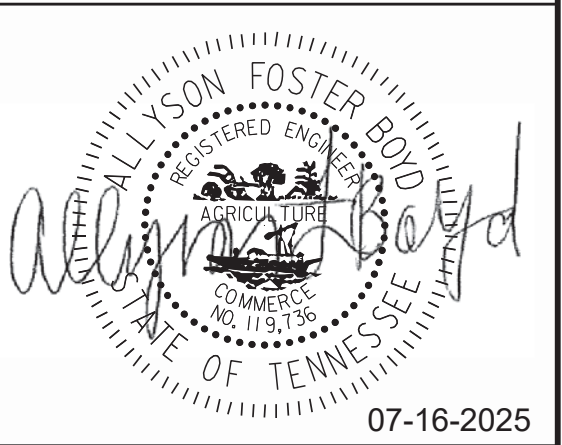
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TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
⬇	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW
●	FLEXIBLE DRUMS (CHANNELIZING)
////////	REMOVE PAVEMENT STRIPING

I-26 UNDER B357
PHASE 2 TRAFFIC CONTROL PLAN

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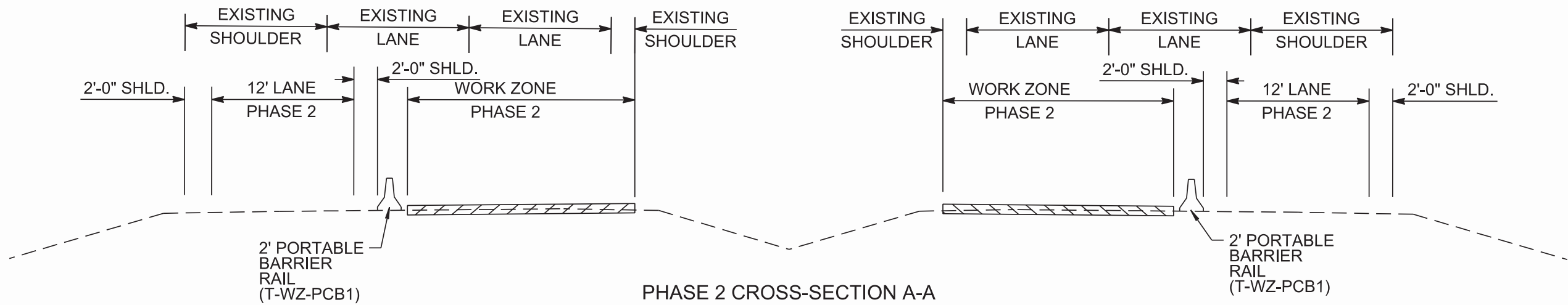


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

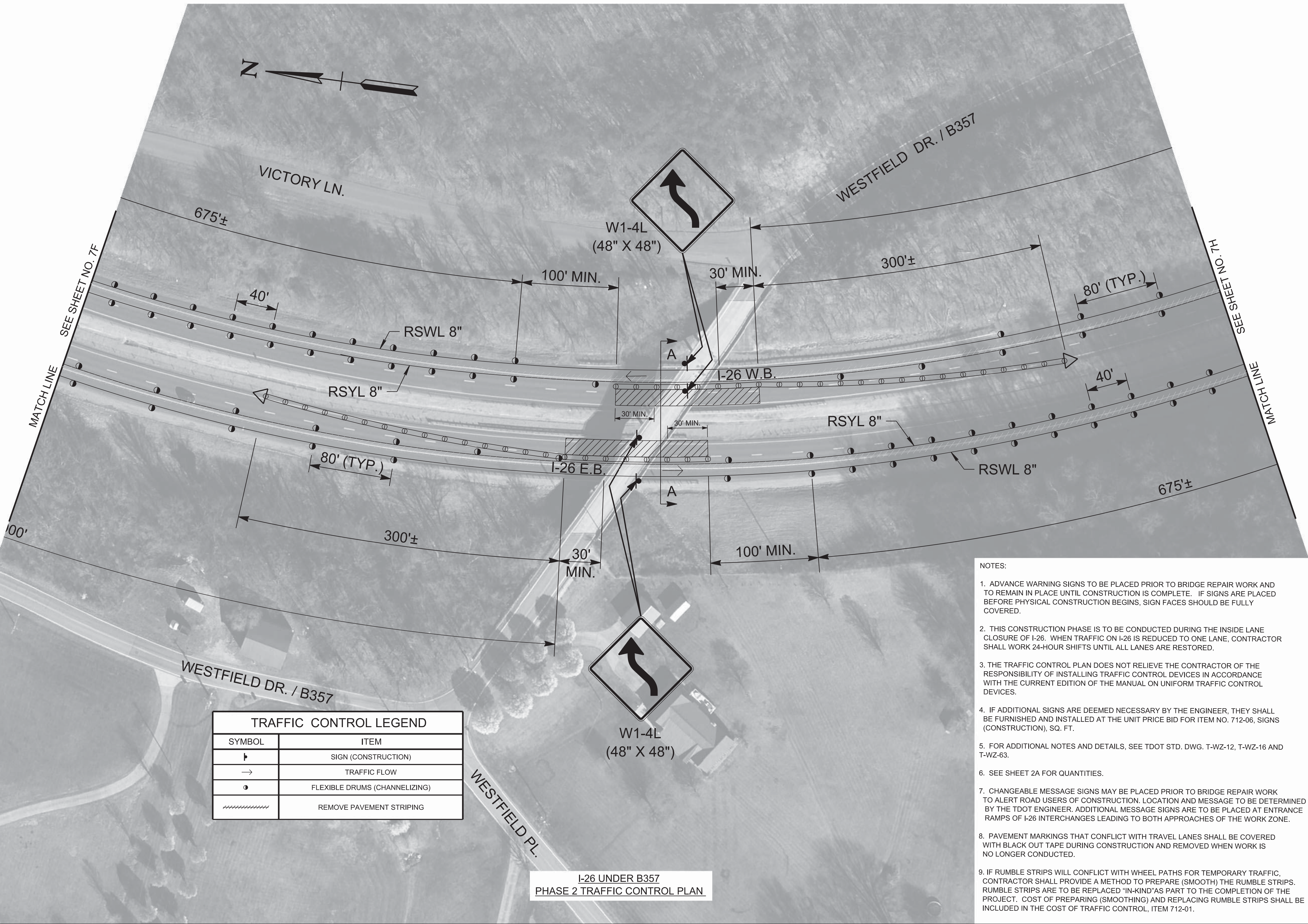
TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 2

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	6G
PS&E	2025	821026-M3-003	7G

SULLIVAN COUNTY WESTFIELD DR.



PHASE 2 CROSS-SECTION A-A

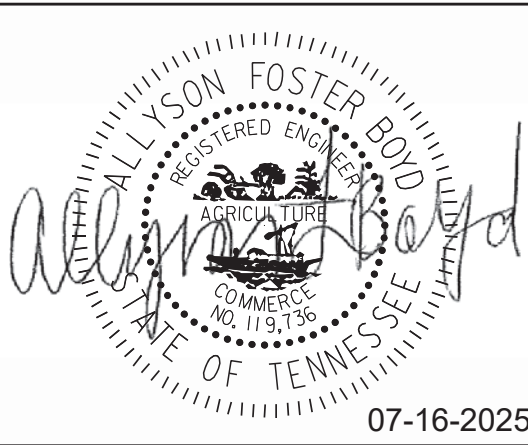


TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
⬮	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW
●	FLEXIBLE DRUMS (CHANNELIZING)
////////	REMOVE PAVEMENT STRIPING

I-26 UNDER B357
PHASE 2 TRAFFIC CONTROL PLAN

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SEALED BY



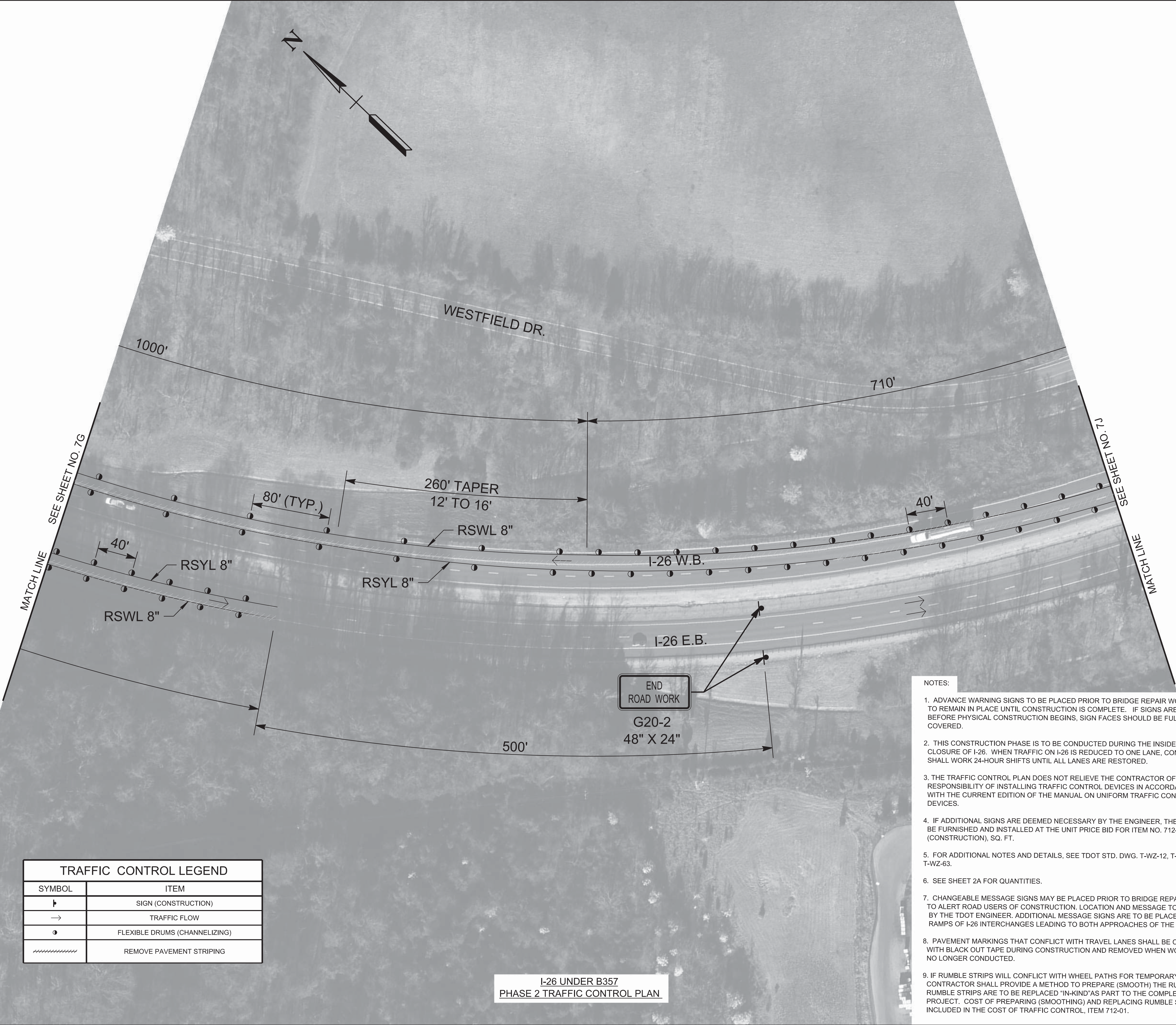
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 2

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	6H
PS&E	2025	821026-M3-003	7H

SULLIVAN COUNTY


WESTFIELD DR.



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
└	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW
●	FLEXIBLE DRUMS (CHANNELIZING)
////////	REMOVE PAVEMENT STRIPING

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SEALED BY



07-16-2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 2

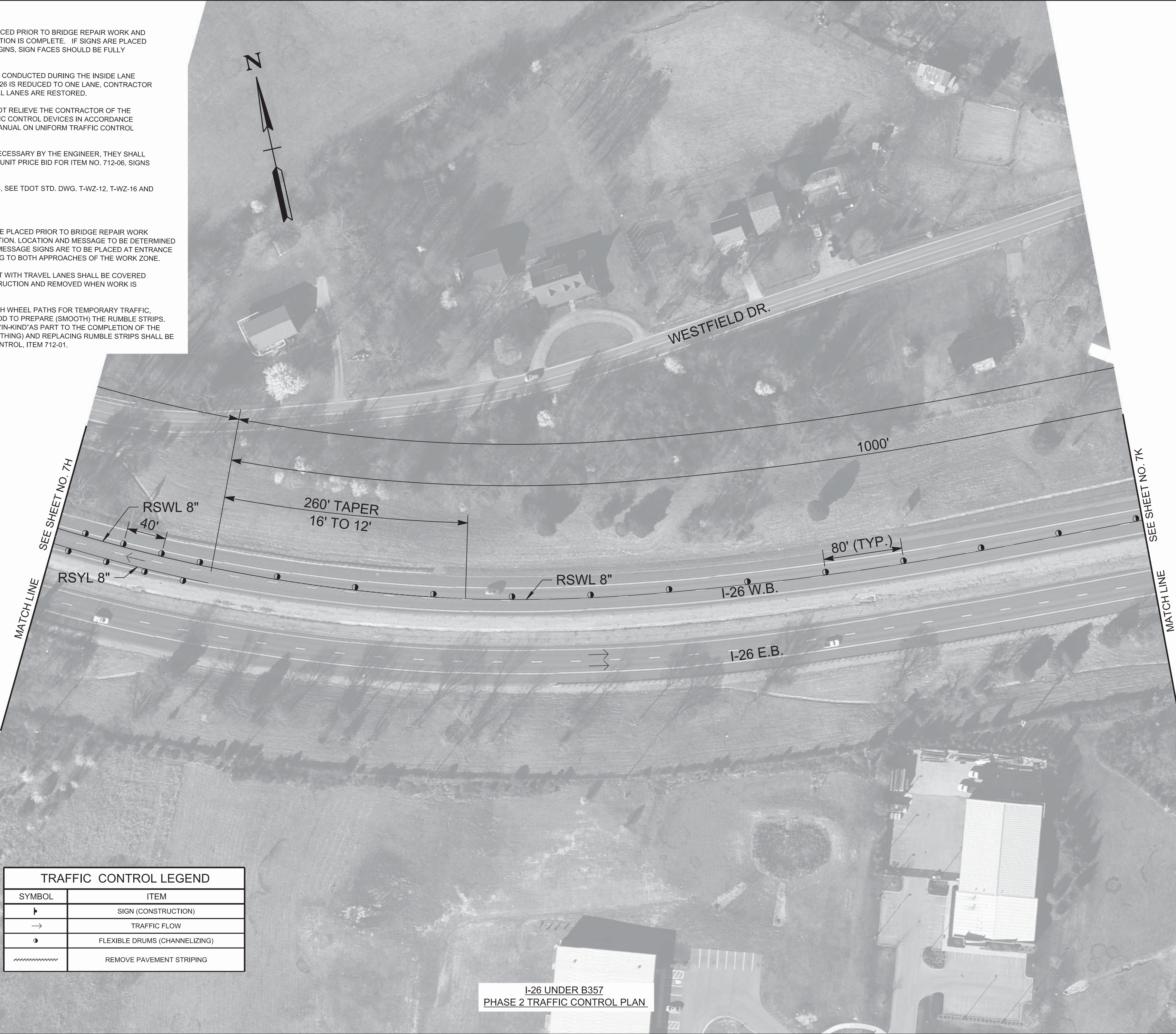
I-26 UNDER B357
PHASE 2 TRAFFIC CONTROL PLAN

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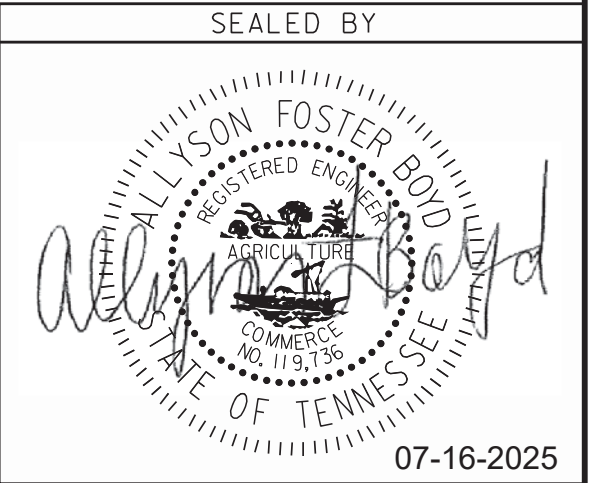
TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	6-I
PS&E	2025	821026-M3-003	7J

SULLIVAN COUNTYWESTFIELD DR.



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
┆	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW
●	FLEXIBLE DRUMS (CHANNELIZING)
////////	REMOVE PAVEMENT STRIPING

I-26 UNDER B357
PHASE 2 TRAFFIC CONTROL PLAN

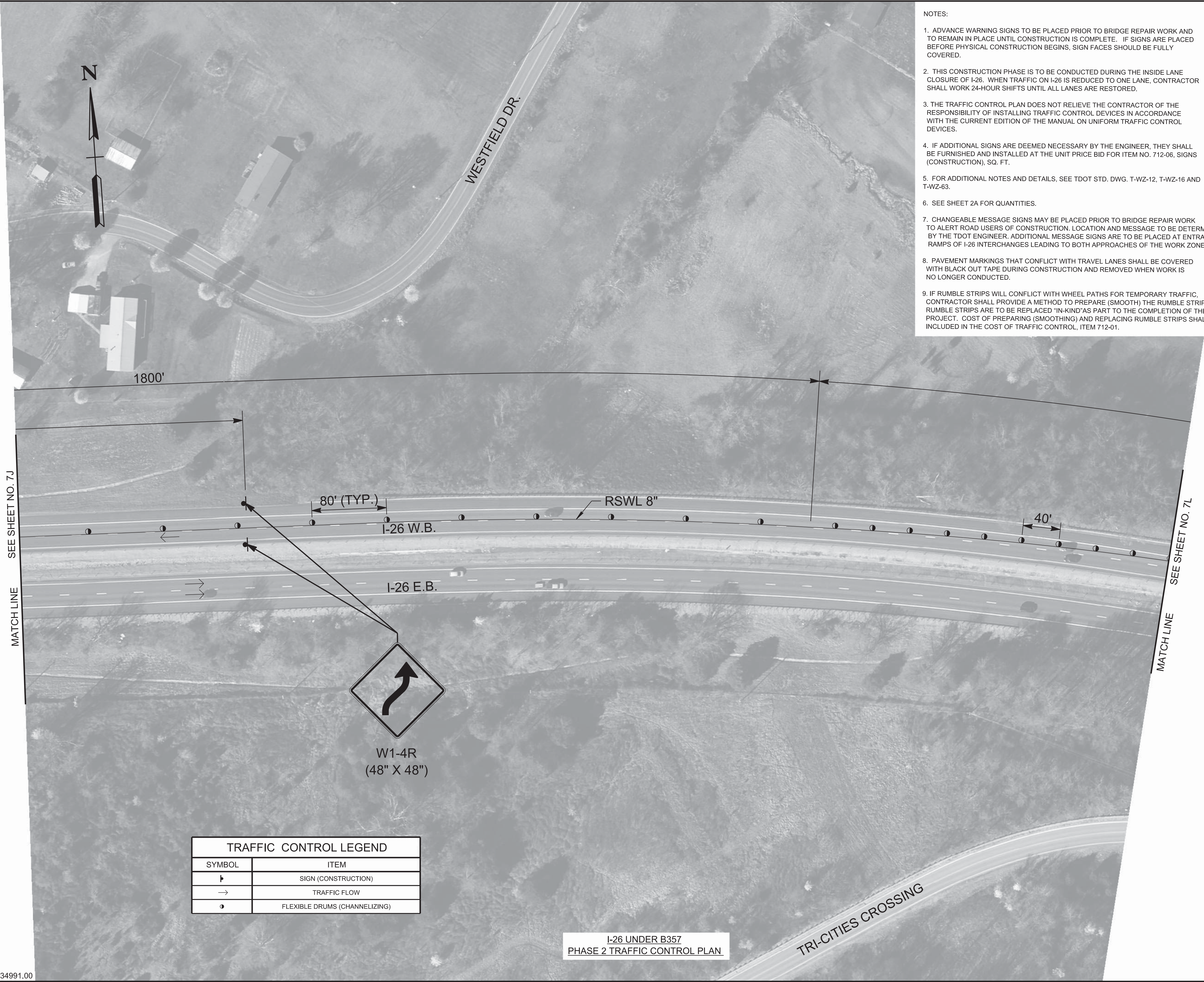


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 2

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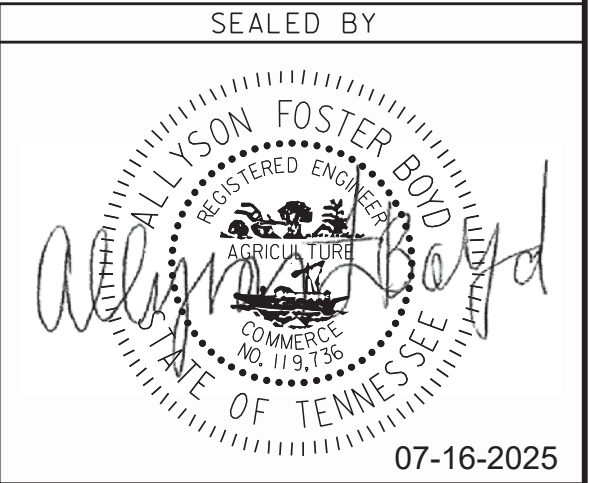
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TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	6J
PS&E	2025	821026-M3-003	7K

SULLIVAN COUNTY

WESTFIELD DR.



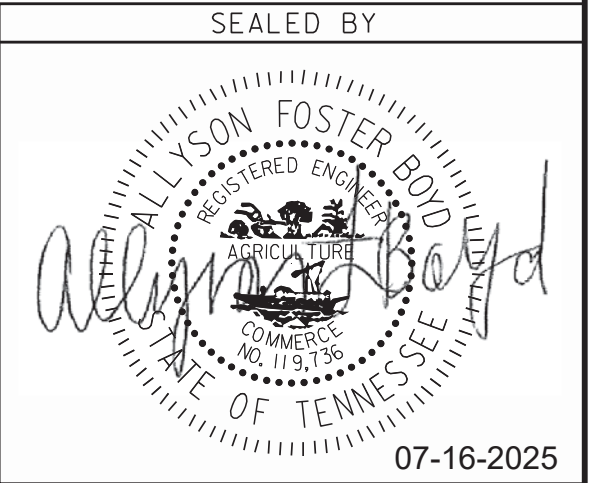
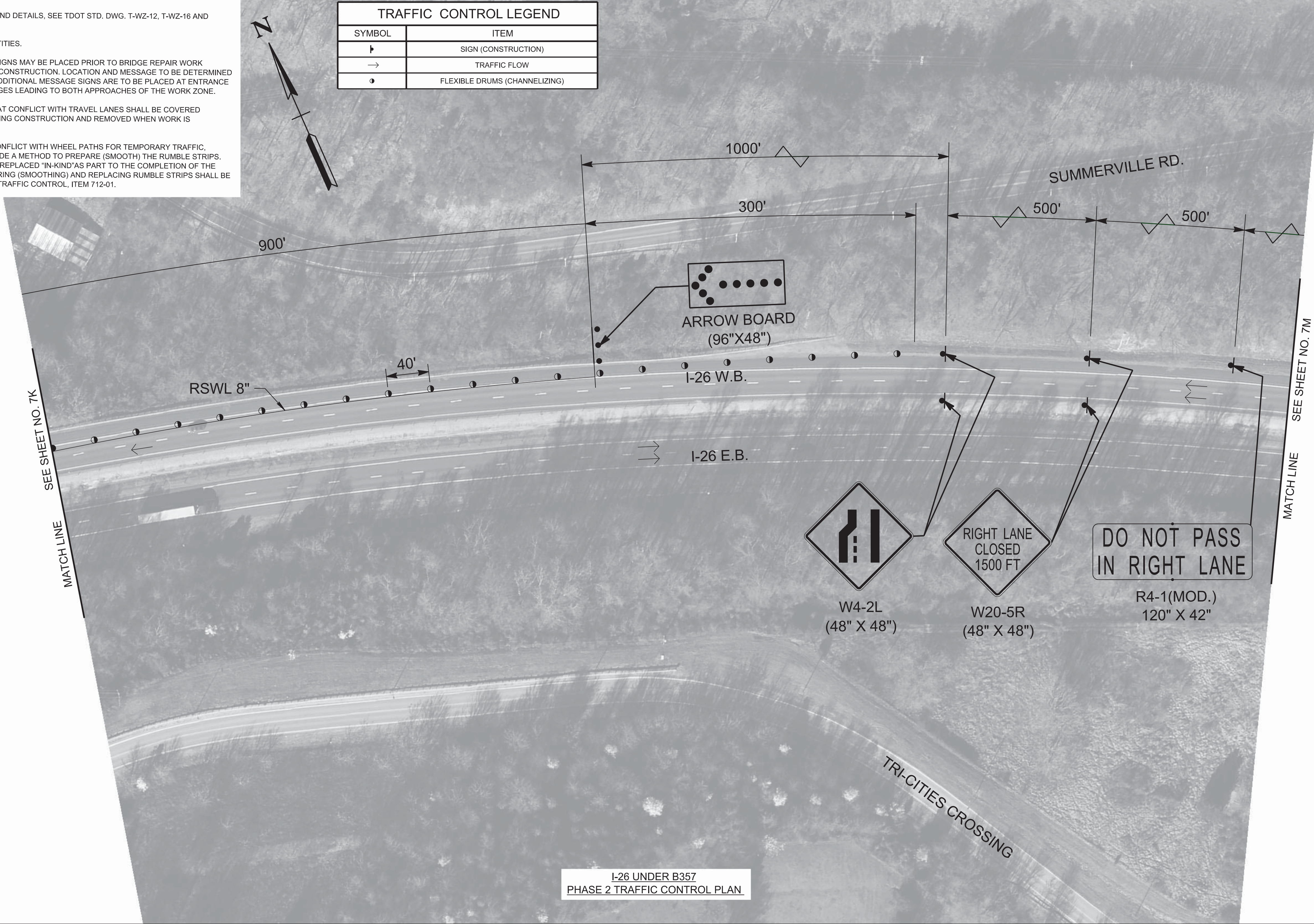
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 2

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TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	6K
PS&E	2025	821026-M3-003	7L

SULLIVAN COUNTYWESTFIELD DR.



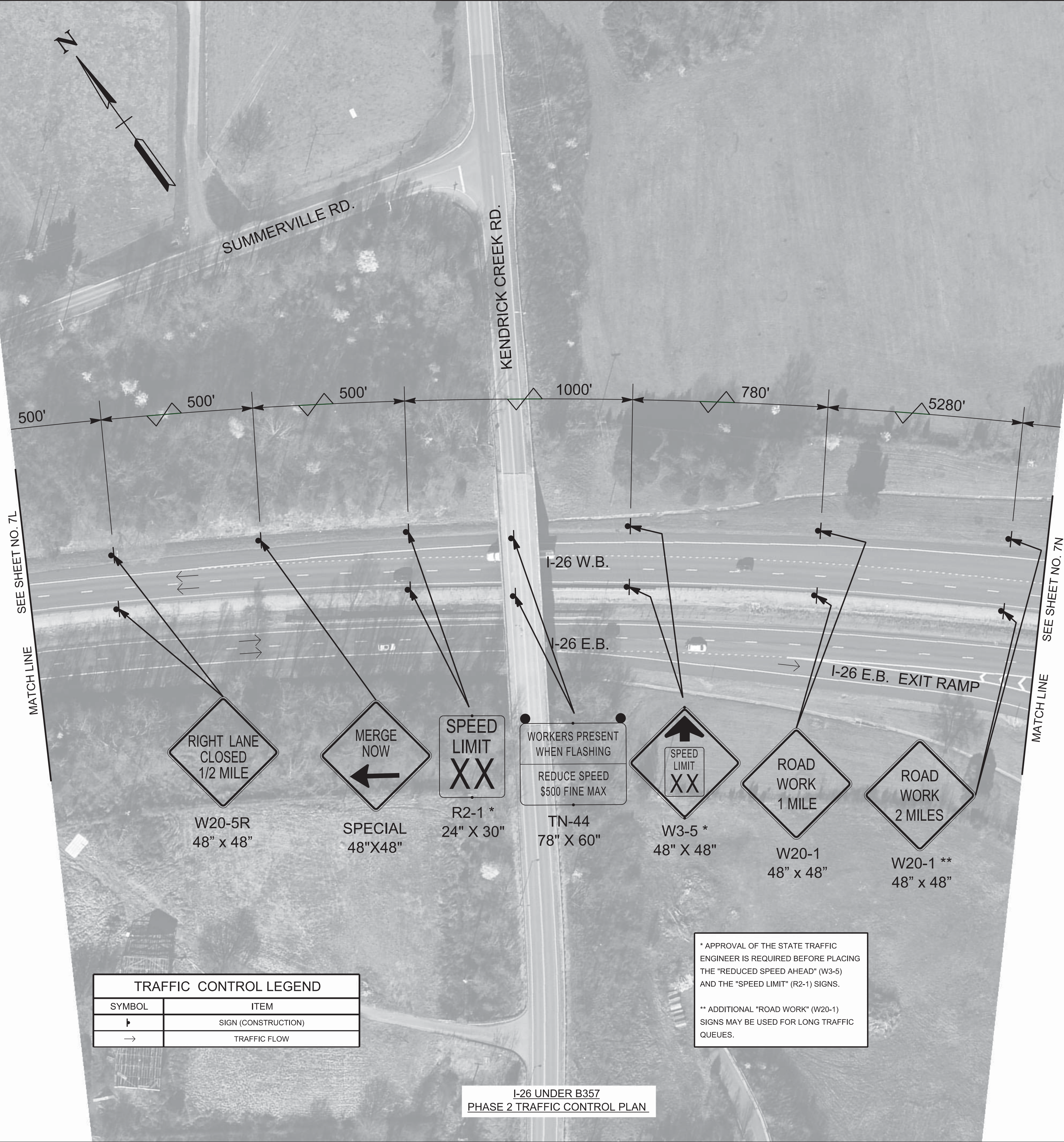
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 2

TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	6L
PS&E	2025	821026-M3-003	7M

SULLIVAN COUNTY

WESTFIELD DR.



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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

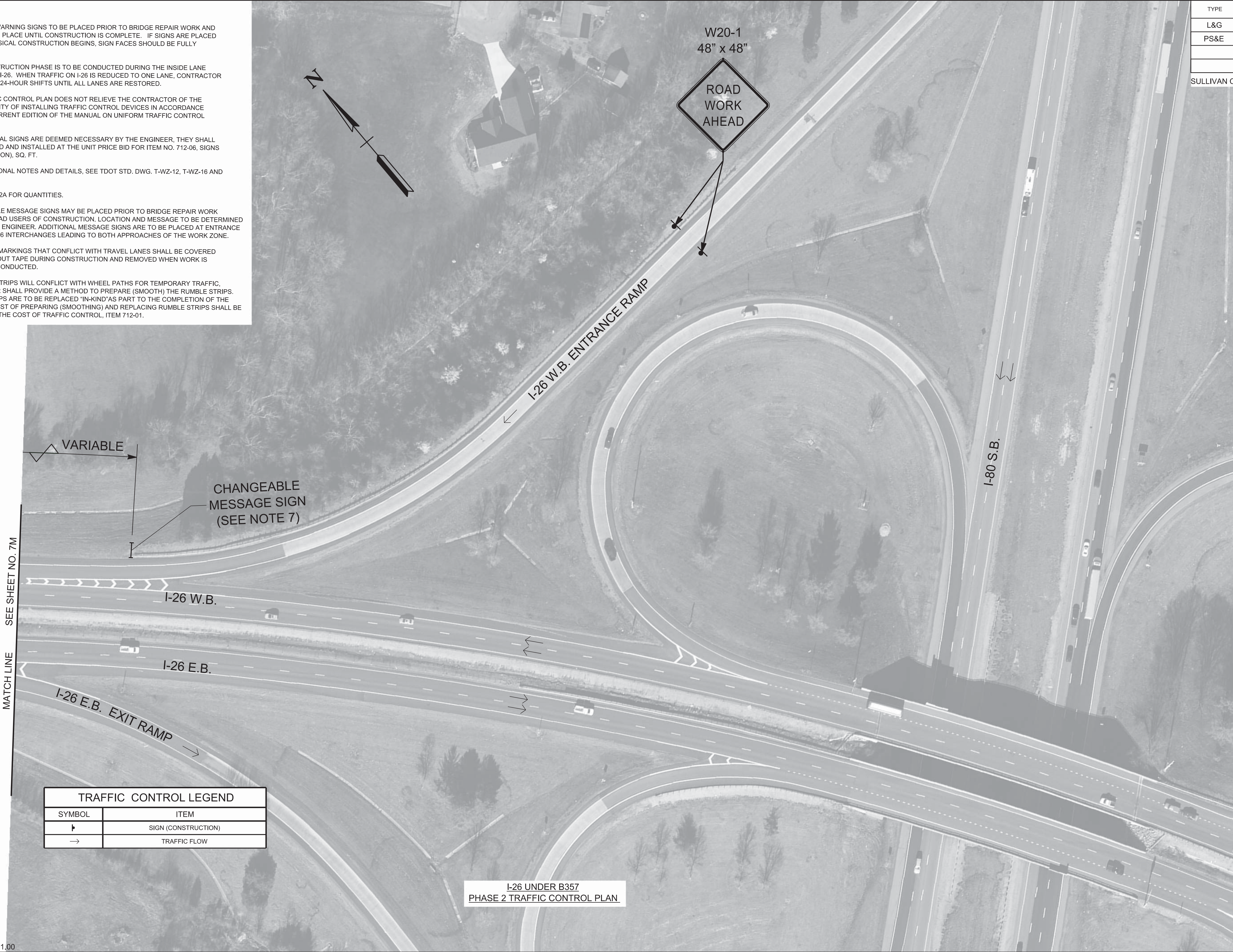
TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 2

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TYPE	YEAR	PROJECT NO.	SHEET NO.
L&G	2025	821026-M3-003	6M
PS&E	2025	821026-M3-003	7N

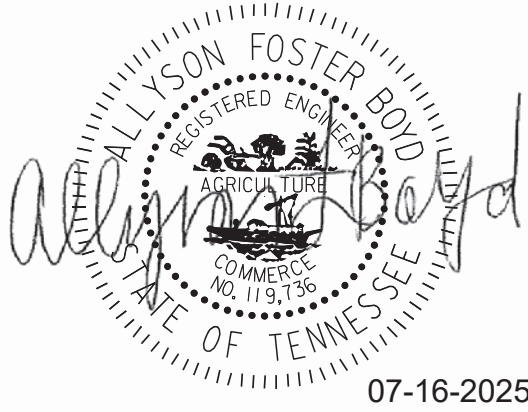
SULLIVAN COUNTYWESTFIELD DR.



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
→	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW

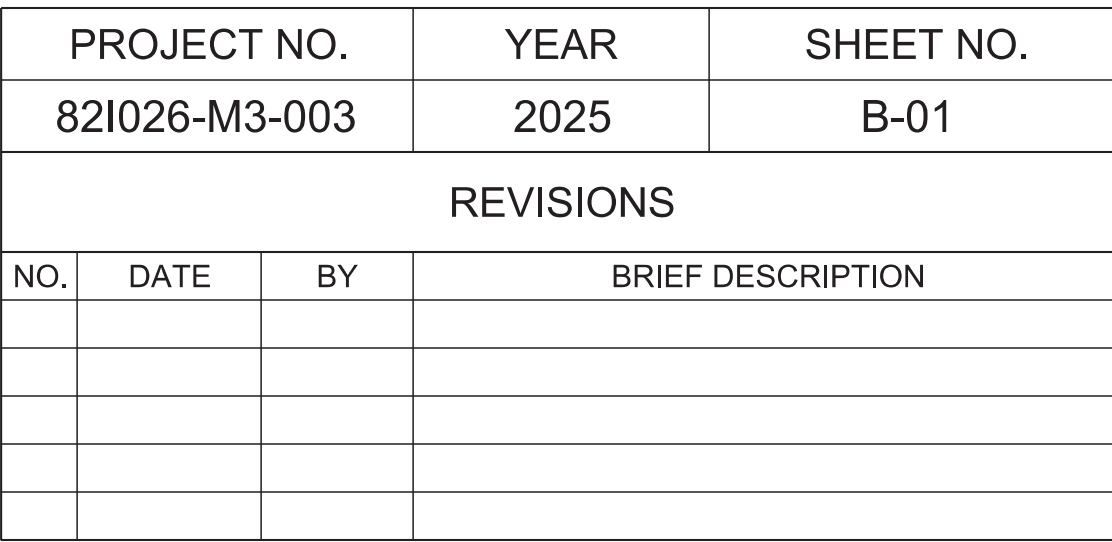
I-26 UNDER B357
PHASE 2 TRAFFIC CONTROL PLAN

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
(BR. NO. 82-B357-0.05)
I-26
PHASE 2



THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS.

DRAWING	SHT. NO.	DWG. NO.	REV. DATE
LAYOUT OF BRIDGE TO BE REPAIRED	B-01	BR-133-268	
GENERAL NOTES AND ESTIMATED BRIDGE QUANTITIES	B-02	BR-133-269	
PHASE CONSTRUCTION	B-03	BR-133-270	
SUPERSTRUCTURE REPAIRS	B-04	BR-133-271	
BEAM REPAIRS	B-05	BR-133-272	
ABUTMENT REPAIRS	B-06	BR-133-273	
BENT 1 REPAIRS	B-07	BR-133-274	
BENT 2 REPAIRS	B-08	BR-133-275	
BENT 3 REPAIRS	B-09	BR-133-276	
JOINT REPLACEMENT AND EPOXY OVERLAY DETAILS	B-10	BR-133-277	
CONCRETE REPAIR DETAILS	B-11	BR-133-278	



K-68-63 THRU K-68-68, STD-1-1

<u>DRAWING</u>	<u>REV. DATE</u>	<u>DESCRIPTION</u>
STD-10-3	01/10/2024	STANDARD FLUME DETAILS

<u>DRAWING</u>	<u>REV. DATE</u>	<u>DESCRIPTION</u>
STD-10-3	01/10/2024	STANDARD FLUME DETAILS

<u>DRAWING</u>	<u>DESCRIPTION</u>	<u>REV. DATE</u>
SP712PTQ	TRAFFIC QUEUE PROTECTION	10/07/2024

<u>DRAWING</u>	<u>DESCRIPTION</u>	<u>REV. DATE</u>
SP712PTQ	TRAFFIC QUEUE PROTECTION	10/07/2024

1. PROVIDE REQUIRED TRAFFIC CONTROL AND PHASE CONSTRUCTION MAINTAINING AT LEAST ONE LANE OF SIGNALIZED TRAFFIC ALONG WESTFIELD ROAD AT ALL TIMES AND AT LEAST ONE 16 FOOT LANE AT ALL TIMES IN EACH DIRECTION ON I-26 BELOW BRIDGE. PORTABLE SIGNAL TO BE UTILIZED ON WESTFIELD ROAD.
2. REMOVE SLIDING PLATE EXPANSION JOINTS AT BOTH ABUTMENTS AND REPLACE WITH COMPRESSION SEALS.
3. RECONSTRUCT THE CONCRETE BRIDGE DECKS IN AREAS OF FULL AND PARTIAL DEPTH DECK REPAIR UTILIZING 18 HOUR HIGH EARLY STRENGTH CONCRETE.
4. SHOTBLAST BRIDGE DECK TO REMOVE ANY OIL, DIRT, RUBBER, TRAFFIC STRIPPING, OR ANY OTHER DETRIMENTAL MATERIAL.
5. PLACE TYPE 1 THIN EPOXY OVERLAY SYSTEM OVER ENTIRE DECK.
6. CLEAN AND PAINT EXISTING STEEL BEARINGS AT ABUTMENTS UTILIZING SYSTEM B.
7. CONSTRUCT ASPHALT PAVEMENT TRANSITIONS AT THE BEGIN AND END OF BRIDGES.
8. REMOVE EXISTING END OF BRIDGE DRAINS TO FACILITATE PLACEMENT OF GUARDRAIL POSTS.

11. GROUT EXISTING HOLES IN WINGPOSTS. DRILL HOLES FOR NEW GUARDRAIL ATTACHMENT UTILIZING MICHIGAN END SHOE DETAILS ON STD. DWG. S-GR31-1A AND PLATE DETAIL SHOWN ON STD-1-1.
12. PROVIDE RIP-RAP FLUME (STD-10-3) ON LEFT AND RIGHT SIDE OF BEGIN BRIDGE. ROUTE FLUMES TO EXISTING DITCHES.
13. PAVE SHOULDER AT RIGHT SIDE END BRIDGE TO ELIMINATE SLOUGHING AND PROVIDE RIP-RAP FLUME ROUTED TO EXISTING DITCH.
14. REPAIR DETERIORATED AREAS OF CONCRETE ON PARAPETS, BEAMS, AND SUBSTRUCTURE UNITS.
15. EPOXY INJECT CRACKS ON BEAMS, AND SUBSTRUCTURE UNITS.
16. HIGH PRESSURE WATER WASH ALL EXPOSED EXISTING CONCRETE SURFACES AND TEXTURE COAT ALL EXPOSED CONCRETE SURFACES OF BENTS, ABUTMENTS, WINGWALLS, PARAPETS, DECK OVERHANGS, AND GIRDERS. TOP AND INSIDES FACE OF PARAPET SHALL BE WHITE. ALL OTHER SURFACES SHALL BE MOUNTAIN GREY.
17. PLACE NEW STRIPING AND SIGNING AS APPROPRIATE.
18. REMOVE VEGETATION FOR A DISTANCE OF TEN FEET ALONG EACH SIDE OF THE BRIDGE (COST TO BE INCLUDED IN ITEM NO. 201-07.01).

I-26 TRAFFIC DATA	
POSTED SPEED LIMIT	= 65 M.P.H
AADT (2023) EB	= 22,945
AADT (2023) WB	= 24,524

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
LAYOUT OF
BRIDGE TO BE REPAIRED

BRIDGE NO. 82-0B357-0.05
FEDERAL BRIDGE ID NO.
82I00260013
WESTFIELD DRIVE (B357)
OVER I-26, LM 0.05
SULLIVAN COUNTY
2025

BR-133-268

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GENERAL NOTES

SPECIFICATIONS & LOADING

CONSTRUCTION SPECIFICATIONS: STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION (JANUARY 1, 2021 EDITION), AND THE 4" EDITION (2017) AASHTO LRFD BRIDGE CONSTRUCTION SPECIFICATIONS WITH INTERIMS.

DESIGN SPECIFICATIONS: 9" EDITION (2020) AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS WITH INTERIMS, AND THE 2" EDITION (2011) AASHTO GUIDE SPECIFICATIONS FOR LRFD SEISMIC BRIDGE DESIGN WITH INTERIMS.

STEEL, CONCRETE, REINFORCING, AND FORMING

REINFORCING STEEL: SHALL BE ASTM A615 GRADE 60 UNLESS NOTED OTHERWISE. SEE SECTION 604 AND 907 OF THE STANDARD SPECIFICATIONS.

HIGH EARLY STRENGTH CONCRETE: THE MIX IS TO MEET THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS, CLASS X. THE CEMENT CONTENT SHALL BE A MINIMUM OF 714 LBS. THE WATER-CEMENT RATIO SHALL BE A MAXIMUM OF 0.40. DESIGN AIR CONTENT SHALL BE 6% WITH ±2% ACCEPTANCE RANGE IN THE FIELD. SLUMP SHALL BE 3±1 INCHES. IF USING A TYPE A, F, OR G WATER REDUCER, THE SLUMP SHALL BE MAXIMUM OF 8 INCHES. NO FLY ASH REPLACEMENT WILL BE PERMITTED. THE MINIMUM 28 DAY COMPRESSIVE STRENGTH SHALL BE 3,500 PSI. TRAFFIC SHALL NOT BE PERMITTED ON ANY OF THE REPAIRED AREAS UNTIL TEST SPECIMENS ATTAIN A MINIMUM COMPRESSIVE STRENGTH OF 3,000 PSI AND THE CONCRETE HAS BEEN IN PLACE A MINIMUM OF 18 HOURS.

CONCRETE CURING: ALL CONCRETE IN REPAIR AREAS SHALL BE CURED ACCORDING TO THE STANDARD SPECIFICATIONS.

MISCELLANEOUS GENERAL NOTES

DEMOLITION: THE CONTRACTOR SHALL TAKE SPECIAL CARE TO PROTECT ANY PARTS OF THE STRUCTURE THAT ARE NOT TO BE REMOVED SPECIFICALLY. FOR FULL DEPTH SLAB REMOVAL, EXCEPT OVER BEAMS, THE MAXIMUM HAMMER SIZE IS 90 POUND CLASS. FOR PARTIAL DEPTH SLAB REMOVAL AND ANY WORK OVER THE BEAMS, THE MAXIMUM HAMMER SIZE IS 60 POUND CLASS; CHIPPING HAMMERS OF THE 15 POUND CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL. SAWING OR CUTTING OF THE CONCRETE IS ACCEPTABLE AS LONG AS ANY SPECIFIED PROJECTION OF THE EXISTING REINFORCING STEEL IS MAINTAINED. EXPANSION JOINT REMOVAL SHALL FOLLOW THE SAME RESTRICTIONS AS FULL DEPTH SLAB REMOVAL. ALL DEVICES PROPOSED FOR CONCRETE DEMOLITION SHALL MEET THE APPROVAL OF THE ENGINEER.

THE CONTRACTOR IS NOT ALLOWED TO USE A HYDRAULIC RAM MOUNTED ON A BACKHOE (COMMONLY CALLED A HOE RAM), MINI EXCAVATOR, OR OTHER EQUIPMENT FOR ANY CONCRETE REMOVAL.

QUICK-SET PATCHING MATERIAL: QUICK-SET PATCHING MATERIAL SHALL BE A POLYMER MODIFIED CEMENTITIOUS PATCHING MATERIAL. SEE TDOT QUALIFIED PRODUCTS LIST 13.009 POLY MOD CEMENT STRUCT PATCH VERT & OVER FOR ACCEPTABLE PATCHING MATERIALS.

PAINT: SHALL BE SYSTEM "B" (QPL 3.005) IN ACCORDANCE WITH SECTION 603.06 OF THE STANDARD SPECIFICATIONS. COLOR OF THE FINISH COAT SHALL COMPLY WITH AMS-STD-595A, FEDERAL STANDARD NO. 36440, GRAY. SEE SECTIONS 603 AND 910 OF THE STANDARD SPECIFICATIONS. ALL PRODUCTS USED IN THIS COATING SYSTEM, INCLUDING THINNERS, MUST BE SUPPLIED BY THE SAME MANUFACTURER.

BEARING CLEANING: ALL STEEL BEARINGS SHALL BE CLEANED AND PAINTED. CLEANING SHALL BE IN ACCORDANCE WITH SECTION 603.05(B.1) OF THE STANDARD SPECIFICATIONS. HAND (SSPC-SP 2) OR POWER (SSPC-SP 3) TOOL CLEANING SHALL REMOVE ALL RUST, SCALE, LOOSE PAINT, AND DIRT. THE COLOR OF THE FINISH COAT SHALL COMPLY WITH AMS-STD-595A, FEDERAL STANDARD NO. 36440, GRAY. SEE SECTIONS 603 AND 910 OF THE STANDARD SPECIFICATIONS.

APPLICATION: THE COATING APPLICATOR SHALL FOLLOW THE MANUFACTURER'S PRINTED INSTRUCTIONS AND SHALL HAVE THESE INSTRUCTIONS ON SITE DURING THE COURSE OF THE WORK.

FORMS AND FALSEWORK: CONCRETE FORM WORK, FALSEWORK, AND TEMPORARY SUPPORTS SHALL BE REMOVED FROM THE JOB SITE AFTER WORK IS COMPLETED. COST OF FORMS, FALSEWORK, AND TEMPORARY SUPPORTS SHALL BE INCLUDED IN ITEMS BID ON. THIS WORK SHALL BE COMPLETED BEFORE FINAL PAYMENT IS APPROVED.

CONCRETE SEALER: INSTALL NON-PENETRATING CONCRETE SEALER ON TOP OF THE SUBSTRUCTURE CAP BEAMS COINCIDING WITH EXPANSION JOINT LOCATIONS BEFORE APPLYING TEXTURE-COATING

CONCRETE SHALL BE CLEAN AND DRY BEFORE APPLYING THE CONCRETE SEALER, AND THE THICKNESS OF THE SEALER SHALL BE AS RECOMMENDED BY THE SEALANT MANUFACTURER. ACCEPTABLE CONCRETE SEALERS ARE INCLUDED IN THE QUALIFIED PRODUCTS LIST FOR NON-PENETRATING CONCRETE SEALERS MAINTAINED BY THE DIVISION OF MATERIALS AND TESTS. THE SEALER SHALL BE CLEAR OR SIMILAR TO THE COLOR OF EXISTING CONCRETE SURFACES TO BE SEALED. THE COST OF THE SEALER, COMPLETE AND IN PLACE, SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM NO. 604-10.44, EXPANSION JOINT REPAIRS.

CONTAINMENT AND DISPOSAL: OUR RECORDS SHOW THAT THIS BRIDGE HAS OR HAS HAD LEAD-BASED/CHROMATE-BASED PAINT APPLIED TO IT DURING ITS HISTORY; THEREFORE, THE CONTRACTOR SHALL ASSUME THAT REMNANTS OF THAT PAINT REMAIN ON THE BRIDGE. SEE SECTION 603.13 OF THE STANDARD SPECIFICATIONS FOR SPECIAL PRECAUTIONS THAT MUST BE TAKEN IN THE REMOVAL; CONTAINMENT AND DISPOSAL OF THE SURFACE PREPARATION WASTE AND PAINT REMOVAL MEDIA; AND WORKER AND PUBLIC SAFETY.

SPECIAL NOTE SURFACE PREPARATION FOR PAINT: OUR RECORDS SHOW THAT THIS BRIDGE HAS OR HAS HAD LEAD-BASED/CHROMATE-BASED PAINT APPLIED TO IT DURING ITS HISTORY. THEREFORE, THE CONTRACTOR SHALL ASSUME THAT REMNANTS OF THAT PAINT REMAIN ON THE BRIDGE ALONG WITH THE POSSIBILITY OF THE PRESENCE OF MILLSCALE. CONTRACTOR SHALL BID ACCORDINGLY.

WORKER PROTECTION: OUR MAINTENANCE RECORDS INDICATE THAT THIS BRIDGE WAS ORIGINALLY PAINTED WITH MATERIALS CONTAINING LEAD AND/OR CHROMATES AND THE CONTRACTOR SHALL BE REQUIRED TO PROCEED ACCORDINGLY AND TAKE ALL MANDATORY SAFEGUARDS PRESCRIBED BY THE STATE AND FEDERAL LAW FOR WORKER PROTECTION AND HAZARDOUS MATERIALS DISPOSAL.

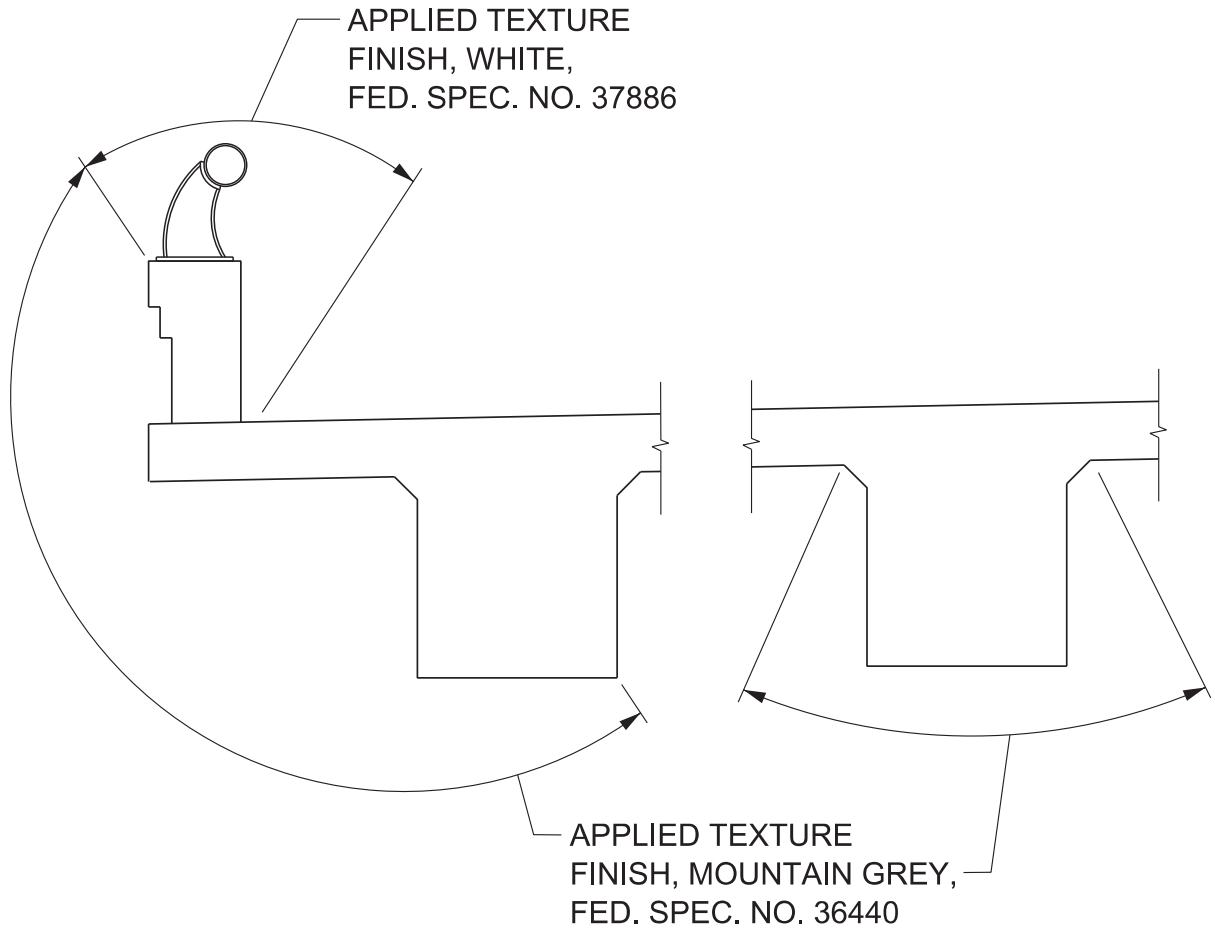
ESTIMATED BRIDGE QUANTITIES				
ITEM NO.	DESCRIPTION	UNIT	82-0B357-0.05	TOTAL
201-07.01	REMOVAL AND DISPOSAL OF BRUSH & TREES	L.S.	1	1
603-02.15	REPAINT EXISTING BEARINGS	L.S.	1	1
603-05.20	CONTAINMENT AND DISPOSAL OF WASTE (BR. NO. 82-0B357-0.05)	L.S.	1	1
604-04.02	APPLIED TEXTURE FINISH (EXISTING STRUCTURES)	S.Y.	1593	1593
604-10.05	CONCRETE	S.F.	12	12
604-10.23	CONCRETE PARAPET REPAIR	L.S.	1	1
604-10.30	BRIDGE DECK REPAIR (FULL DEPTH OF SLAB)	S.Y.	10	10
604-10.44	EXPANSION JOINT REPAIRS	L.F.	60	60
604-10.50	BRIDGE DECK REPAIR (PARTIAL DEPTH OF SLAB)	S.Y.	200	200
604-10.54	CONCRETE REPAIRS	S.F.	12	12
604-10.58	EPOXY INJECTION (INJECTION)	GAL.	14	14
604-10.62	EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE)	L.F.	285	285
617-04.01	TYPE 1 THIN OVERLAY (EPOXY URETHANE)	S.Y.	870	870

* DENOTES ITEM CAN BE INCREASED, DECREASED, OR ELIMINATED AS DIRECTED BY THE ENGINEER.

FOOTNOTES:

- INCLUDES ALL LABOR AND MATERIALS NECESSARY FOR THE SURFACE PREPARATION AND PAINTING OF STEEL BEARING DEVICES AT ABUTMENTS. SEE NOTES ON GENERAL NOTES SHEET.
- INCLUDES ALL LABOR AND MATERIALS NECESSARY FOR CONTAINMENT AND DISPOSAL OF ALL WASTE GENERATED DURING CLEANING AND PAINTING OF STEEL BEARINGS AT ABUTMENTS.
- INCLUDES ALL LABOR AND MATERIALS NECESSARY FOR THE APPLICATION OF ALL TEXTURE COATING FOR THE FULL LENGTH OF THE BRIDGE AS SHOWN IN THE TEXTURE COATING DETAIL ON THIS SHEET. ALSO INCLUDES SURFACE PREPARATION USING A HIGH PRESSURE WASH TO REMOVE ALL LOOSE COATINGS, FLAKING AND OTHER FOREIGN SUBSTANCES TO THE FULL SATISFACTION OF THE ENGINEER.
- INCLUDES ALL LABOR AND MATERIALS NECESSARY TO PLACE HIGH EARLY STRENGTH CONCRETE FOR REPAIR OF INDICATED AREAS.
- INCLUDES ALL LABOR, MATERIALS, AND INCIDENTALS NECESSARY TO GROUT EXISTING HOLES AND RECESS IN THE EXISTING WINGPOSTS TO ALLOW ATTACHMENT OF NEW GUARDRAIL UTILIZING MICHIGAN END SHOE. ALSO INCLUDES THE REPLACEMENT OF TWO MISSING BOLTS AND ONE MISSING ANCHOR AND BOLT ASSEMBLY FROM THE EXISTING PARAPET RAILING.
- INCLUDES THE COST OF ALL LABOR AND MATERIALS NECESSARY TO REMOVE SLIDING PLATES AT ABUTMENTS 1 AND 2 AND REPLACE WITH COMPRESSION SEALS.
- INCLUDES THE COST OF ALL LABOR AND MATERIALS NECESSARY TO PLACE A POLYMER MODIFIED CEMENTITIOUS STRUCTURAL PATCHING MATERIAL FOR REPAIR OF INDICATED AREAS.

PROJECT NO.		YEAR	SHEET NO.
82I026-M3-003		2025	B-02
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



APPLIED TEXTURE FINISH SKETCH

LEFT SIDE ONLY SHOWN, SKETCH IS SYMMETRIC ABOUT C/ BRIDGE.

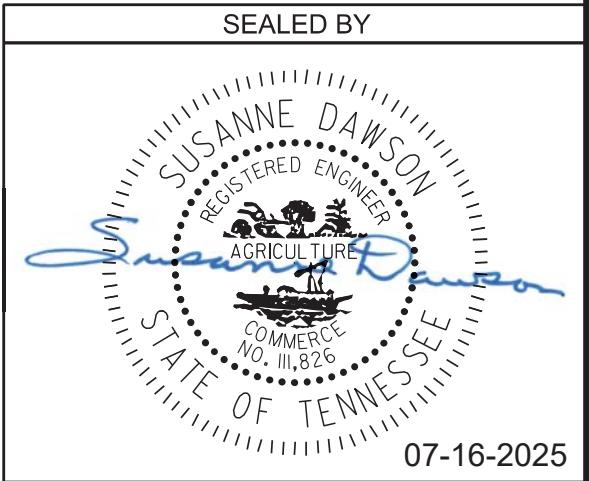
HIGH PRESSURE WATER WASH ALL EXPOSED EXISTING CONCRETE SURFACES AND TEXTURE COAT ALL EXPOSED CONCRETE SURFACES OF PARAPETS, DECK OVERHANGS, GIRDERS, ABUTMENTS, BENTS AND PORTIONS OF WINGWALLS THAT PARALLEL END SLOPES. TOP AND INSIDES FACE OF PARAPETS SHALL BE WHITE (AMS-STD-595A, COLOR NO. 37886). ALL OTHER SURFACES SHALL BE MOUNTAIN GREY (AMS-STD-595A, COLOR NO. 36440).

COST OF TEXTURE COATING SHALL BE INCLUDED IN ITEM NO. 604-04.02, APPLIED TEXTURE FINISH (EXISTING STRUCTURES), S.Y.

THE CONTRACTOR SHALL USE CONTAINMENT SCREENS OR OTHER MEASURES AS NECESSARY TO PREVENT ANY TEXTURE COATING FROM ENTERING THE ENVIRONMENT. CONTAINMENT MEASURES SHALL BE APPROVED BY THE ENGINEER AND COST SHALL BE INCLUDED IN ITEMS BID ON.

THE EXISTING SURFACES THAT ARE TO RECEIVE A TEXTURE FINISH SHALL BE FREE OF ALL EFFLORESCENCE, FLAKING TEXTURE COATING, RUST, DIRT, OIL, AND OTHER FOREIGN SUBSTANCES PRIOR TO THE APPLICATION OF THE TEXTURE FINISH. THE SURFACE SHALL BE CLEANED TO THE COMPLETE SATISFACTION OF THE ENGINEER USING A HIGH PRESSURE WATER WASH. COST TO BE INCLUDED IN ITEM NO. 604-04.02, APPLIED TEXTURE FINISH (EXISTING STRUCTURES), S.Y.

THE WASH WATER IS TO BE FILTERED AND PAINT CHIPS AND DEBRIS COLLECTED PRIOR TO RELEASE OF WATER.



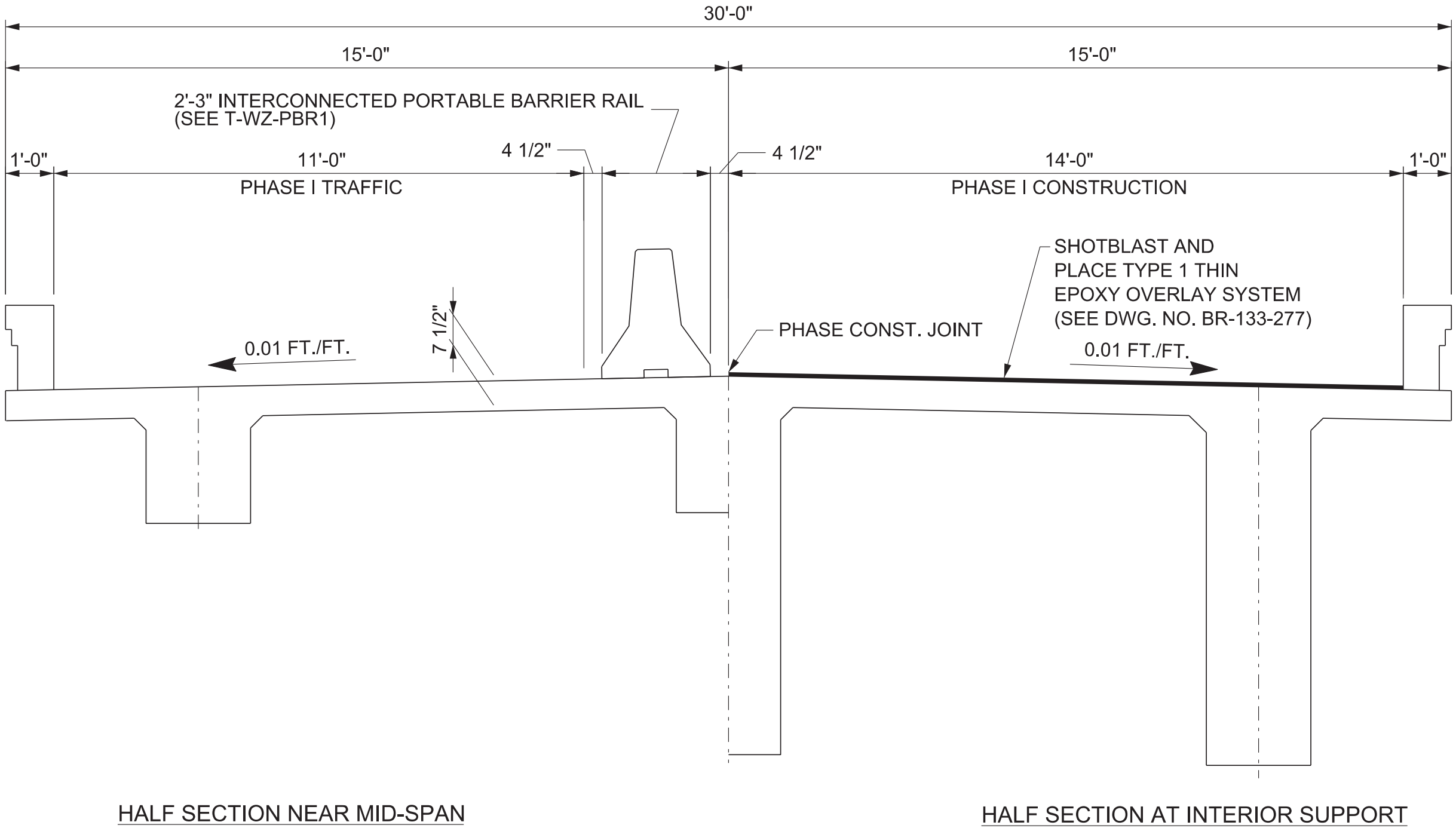
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
GENERAL NOTES AND
ESTIMATED BRIDGE QUANTITIES

BRIDGE NO. 82-0B357-0.05
FEDERAL BRIDGE ID NO.
82I00260013
WESTFIELD DRIVE (B357)
OVER I-26, LM 0.05
SULLIVAN COUNTY
2025

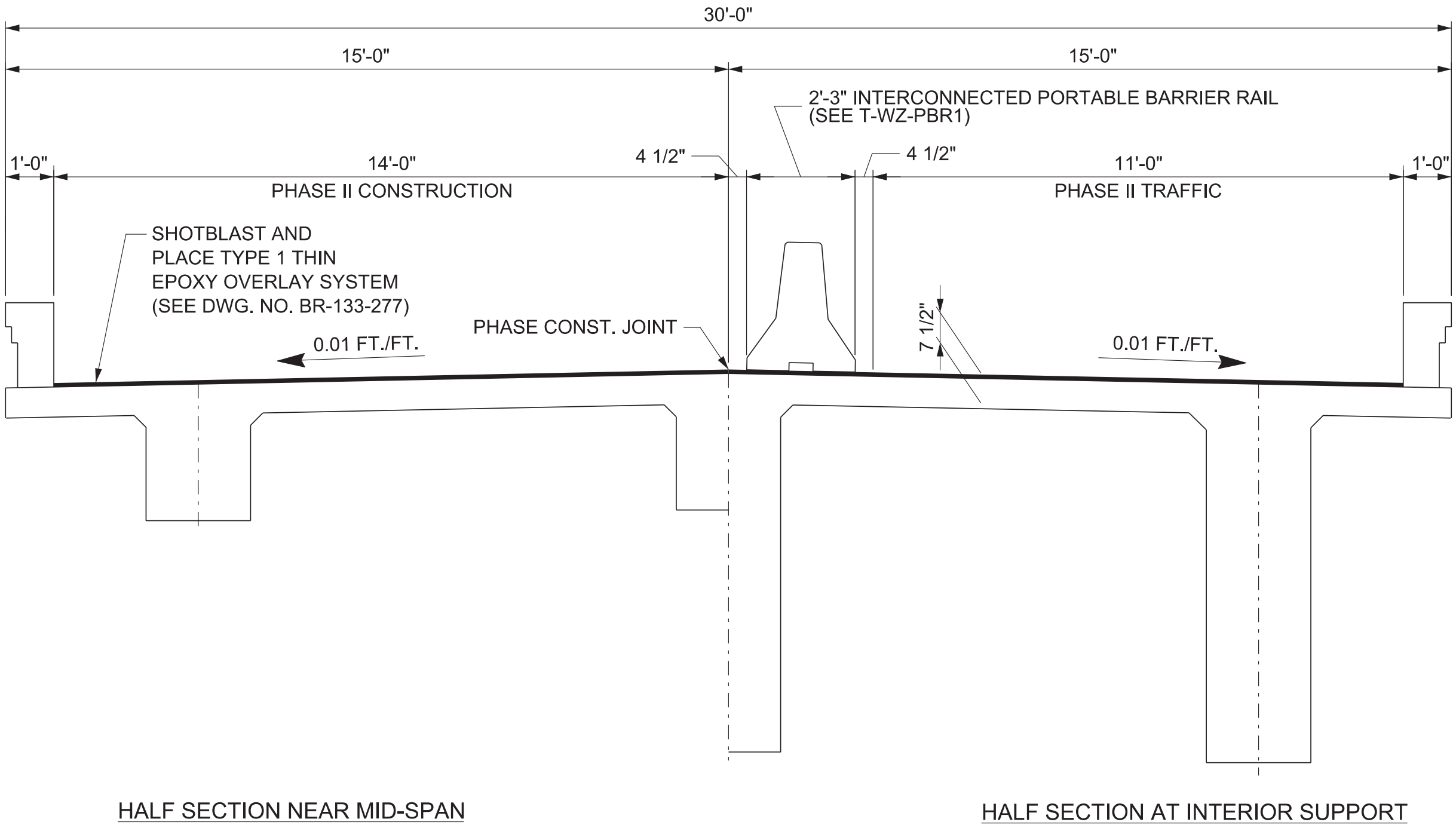
BR-133-269

PIN NO.:	134991.00	
DESIGN BY:	SUSANNE DAWSON	DATE: MARCH 2024
DRAWN BY:	D. PICKEL	DATE: MARCH 2024
SUPERVISED BY:	SUSANNE DAWSON	DATE: MARCH 2024
CHECKED BY:	FRANK BALE	DATE: MARCH 2024

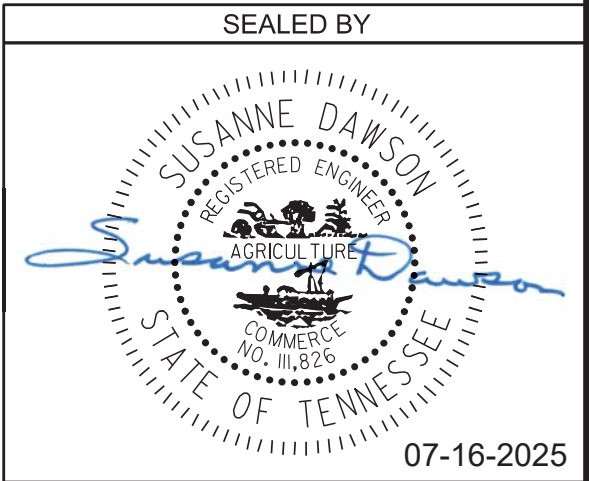
PROJECT NO.		YEAR		SHEET NO.	
82I026-M3-003		2025		B-03	
REVISIONS					
NO.	DATE	BY	BRIEF DESCRIPTION		



WESTFIELD DRIVE PHASE I CONSTRUCTION
(LOOKING FORWARD ON SURVEY)



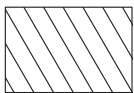
WESTFIELD DRIVE PHASE II CONSTRUCTION
(LOOKING FORWARD ON SURVEY)




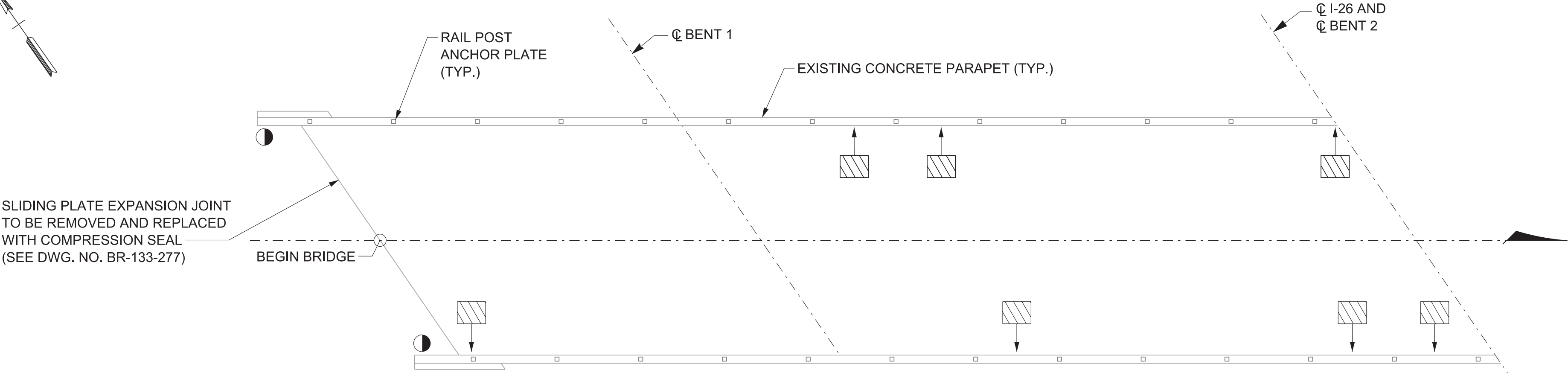
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
PHASE CONSTRUCTION
BRIDGE NO. 82-0B357-0.05
FEDERAL BRIDGE ID NO.
82I00260013
WESTFIELD DRIVE (B357)
OVER I-26, LM 0.05
SULLIVAN COUNTY
2025
BR-133-270

PIN NO.: 134991.00
DESIGN BY: SUSANNE DAWSON
DRAWN BY: D. PICKEL
SUPERVISED BY: SUSANNE DAWSON
CHECKED BY: FRANK BALE
DATE: MARCH 2024
DATE: MARCH 2024
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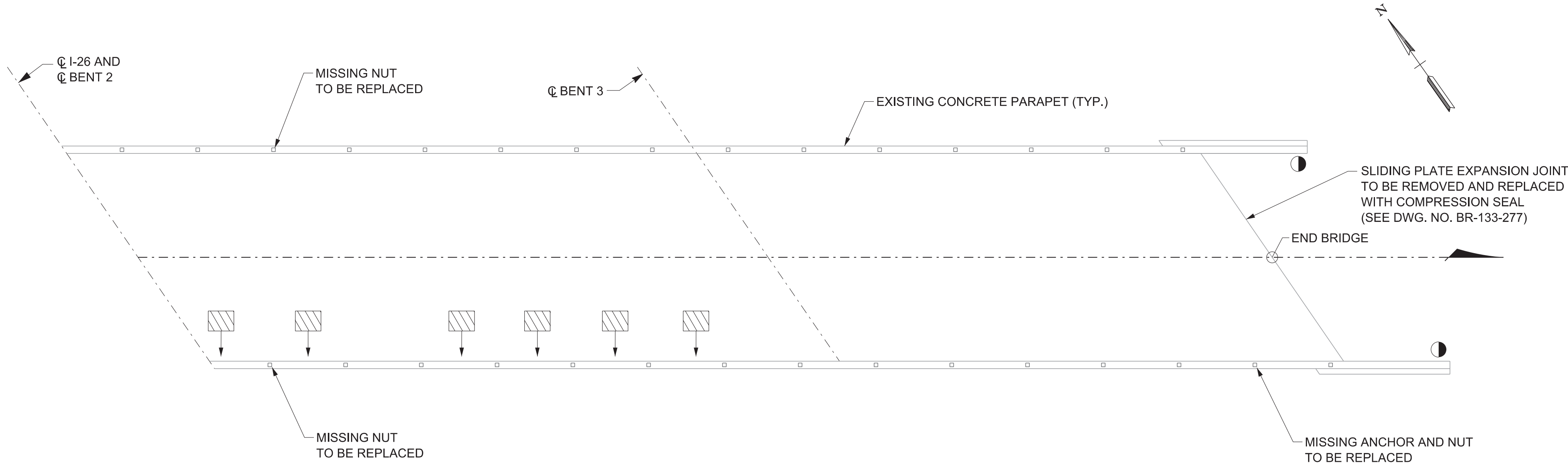
PROJECT NO.		YEAR	SHEET NO.
82I026-M3-003		2025	B-04
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

 DENOTES AREAS TO BE REPAIRED UNDER ITEM NO. 604-10.05 AND/OR 604-10.54, CONCRETE/CONCRETE REPAIRS. SEE CONCRETE REPAIR DETAILS ON DWG. NO. BR-133-278.

 DENOTES LOCATION OF GUARDRAIL ATTACHMENT MODIFICATIONS TO BE PAID FOR UNDER ITEM NO. 604-10.23, CONCRETE PARAPET REPAIR. SEE DWG. NO. BR-133-279 FOR STD. DWG. STD-1-1.



PLAN
SUPERSTRUCTURE REPAIRS
BEGIN BRIDGE TO BENT 2
NOT TO SCALE



PLAN
SUPERSTRUCTURE REPAIRS
BENT 2 TO END BRIDGE
NOT TO SCALE

ESTIMATED QUANTITIES		
LOCATION OF REPAIR	APPROX. REPAIR AREAS ITEM NO. 604-10.05 AND/OR 604-10.54, CONCRETE/CONCRETE REPAIRS (S.F.)	APPROX. REPAIR AREAS ITEM NO. 604-10.62, EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE) (L.F.)
SUPERSTRUCTURE	19 *	196 *

* INCLUDES BEAM REPAIRS SHOWN ON DWG. NO. BR-133-272.

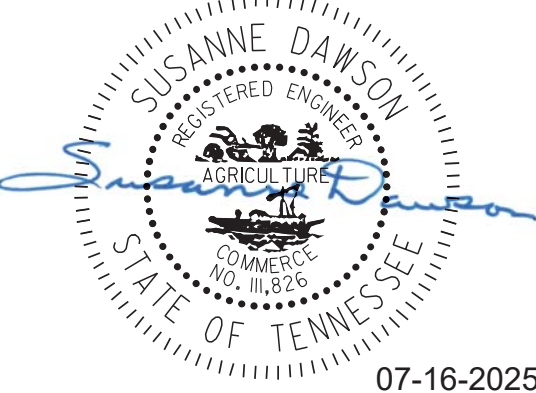
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE REPAIRS

BRIDGE NO. 82-0B357-0.05
FEDERAL BRIDGE ID NO.
82I00260013
WESTFIELD DRIVE (B357)
OVER I-26, LM 0.05
SULLIVAN COUNTY
2025

BR-133-271


SEALED BY




07-16-2025

PIN NO.:	134991.00	
DESIGN BY:	SUSANNE DAWSON	DATE: MARCH 2024
DRAWN BY:	D. PICKEL	DATE: MARCH 2024
SUPERVISED BY:	SUSANNE DAWSON	DATE: MARCH 2024
CHECKED BY:	FRANK BALE	DATE: MARCH 2024

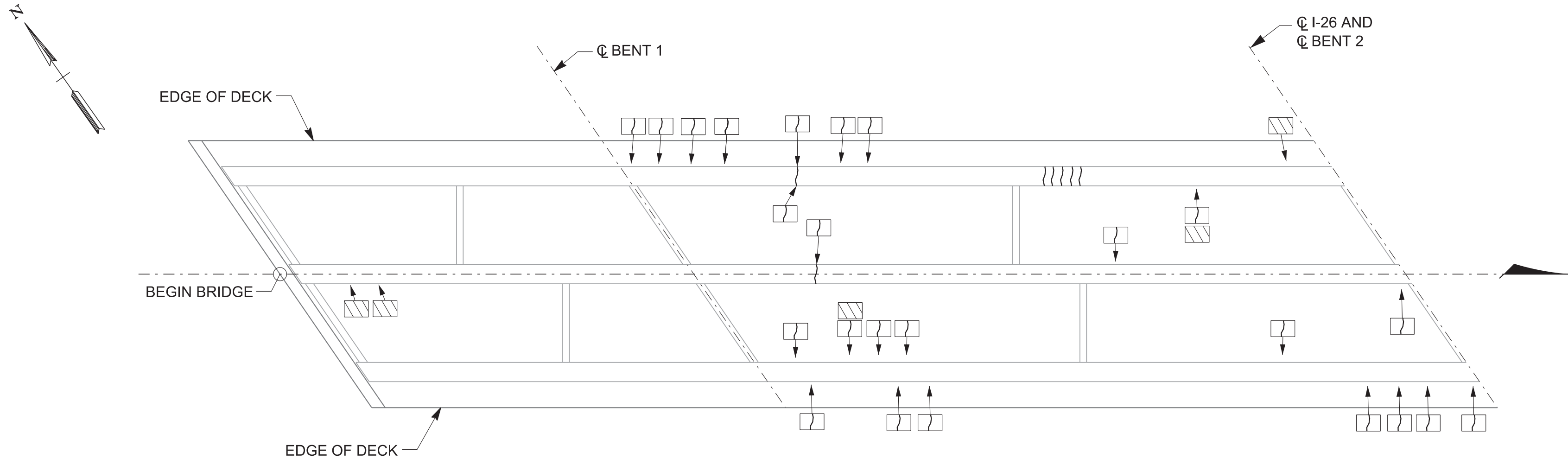
PROJECT NO.			YEAR	SHEET NO.
82I026-M3-003			2025	B-05
REVISIONS				
NO.	DATE	BY	BRIEF DESCRIPTION	

 DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 AND/OR 604-10.54, CONCRETE/CONCRETE REPAIRS. SEE CONCRETE REPAIR DETAILS ON DWG. NO. BR-133-278.

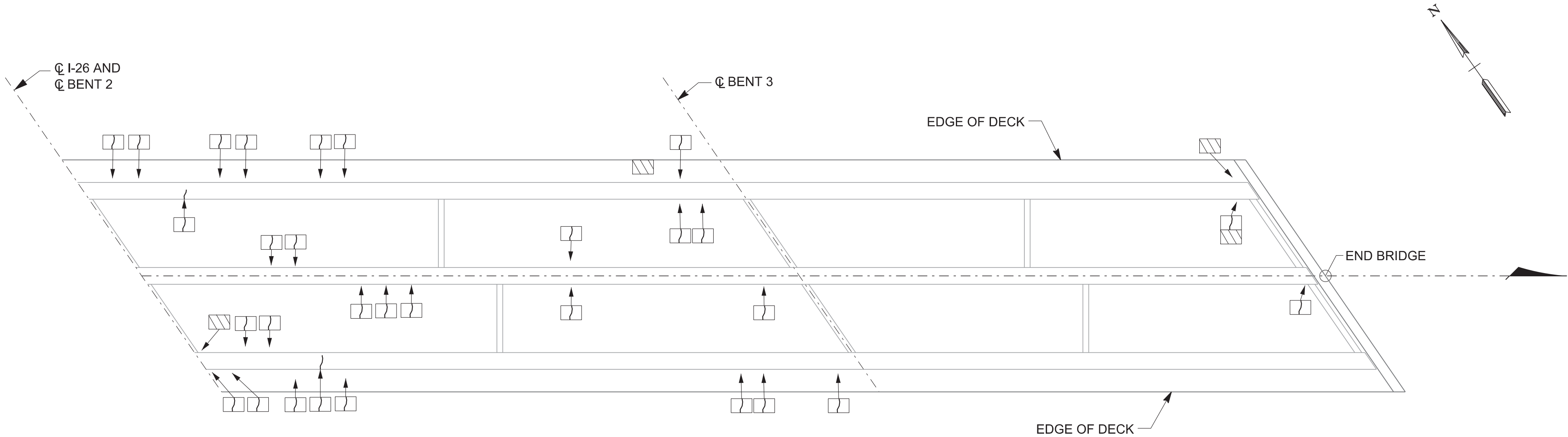
 DENOTES CRACK TO BE EPOXY INJECTED. QUANTITIES AND LOCATIONS SHOWN ARE APPROXIMATE AND MAY BE INCREASED, DECREASED OR ELIMINATED BY THE ENGINEER. SEE EPOXY INJECTION NOTES ON DWG. NO. BR-133-278.

SEE SHEET BR-133-271 FOR REPAIR QUANTITIES.

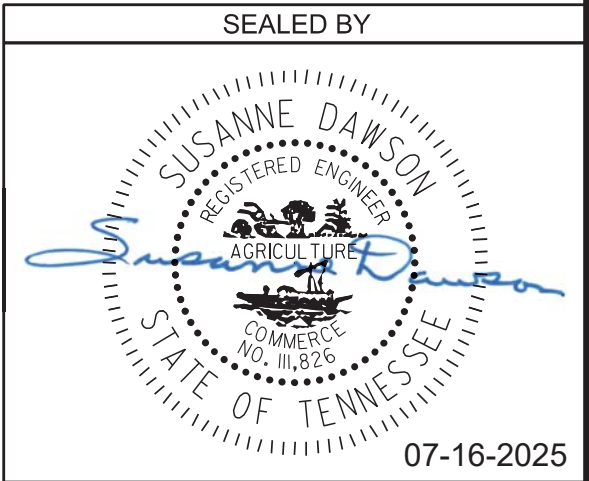
ALL REPAIRS SHOWN ON THIS SHEET
ARE BELOW DECK
UNLESS OTHERWISE NOTED



PLAN
BEAM REPAIRS
BEGIN BRIDGE TO BENT 2
NOT TO SCALE



PLAN
BEAM REPAIRS
BENT 2 TO END BRIDGE
NOT TO SCALE



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BEAM REPAIRS
BRIDGE NO. 82-0B357-0.05
FEDERAL BRIDGE ID NO.
82I00260013
WESTFIELD DRIVE (B357)
OVER I-26, LM 0.05
SULLIVAN COUNTY
2025

BR-133-272

PIN NO.: 134991.00
DESIGN BY: SUSANNE DAWSON DATE: MARCH 2024
DRAWN BY: D. PICKEL DATE: MARCH 2024
SUPERVISED BY: SUSANNE DAWSON DATE: MARCH 2024
CHECKED BY: FRANK BALE DATE: MARCH 2024

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PIN NO.:134991.00

DESIGN BY:SUSANNE DAWSON

DRAWN BY:D. PICKEL

SUPERVISED BY:SUSANNE DAWSON


CHECKED BY:FRANK BALE

DATE: MARCH 2024

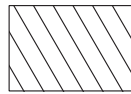
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DATE: MARCH 2024



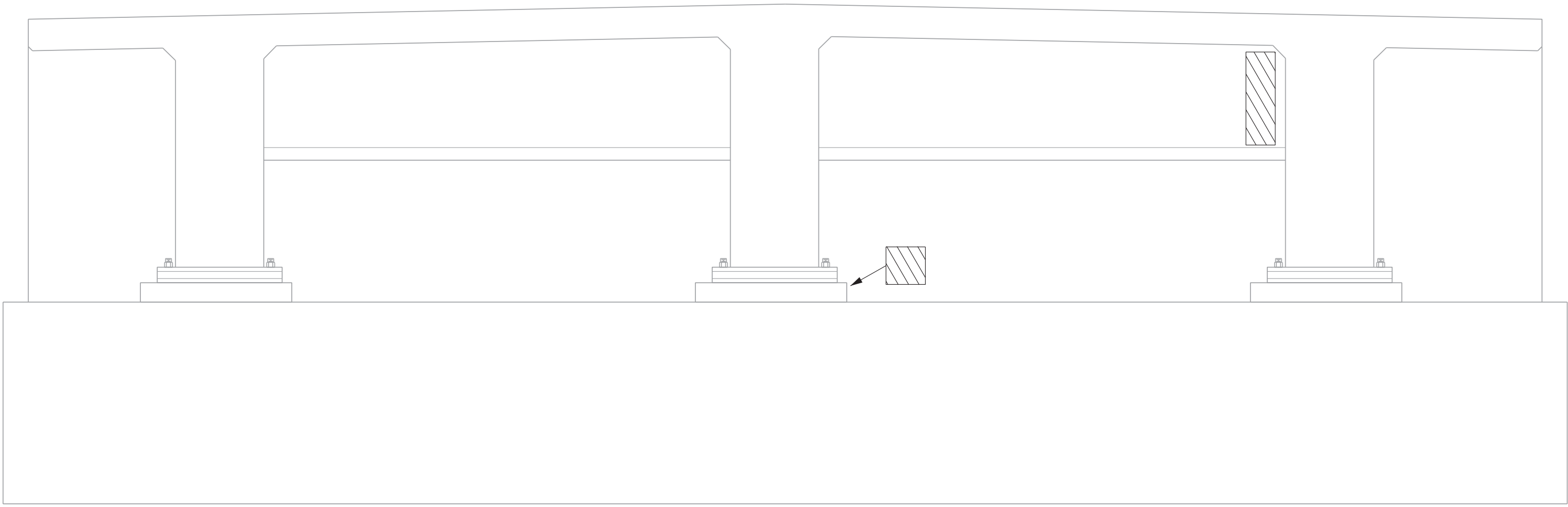
DENOTES CRACK TO BE EPOXY INJECTED. QUANTITIES AND LOCATIONS SHOWN ARE APPROXIMATE AND MAY BE INCREASED, DECREASED OR ELIMINATED BY THE ENGINEER. SEE EPOXY INJECTION NOTES ON DWG. NO. BR-133-278.



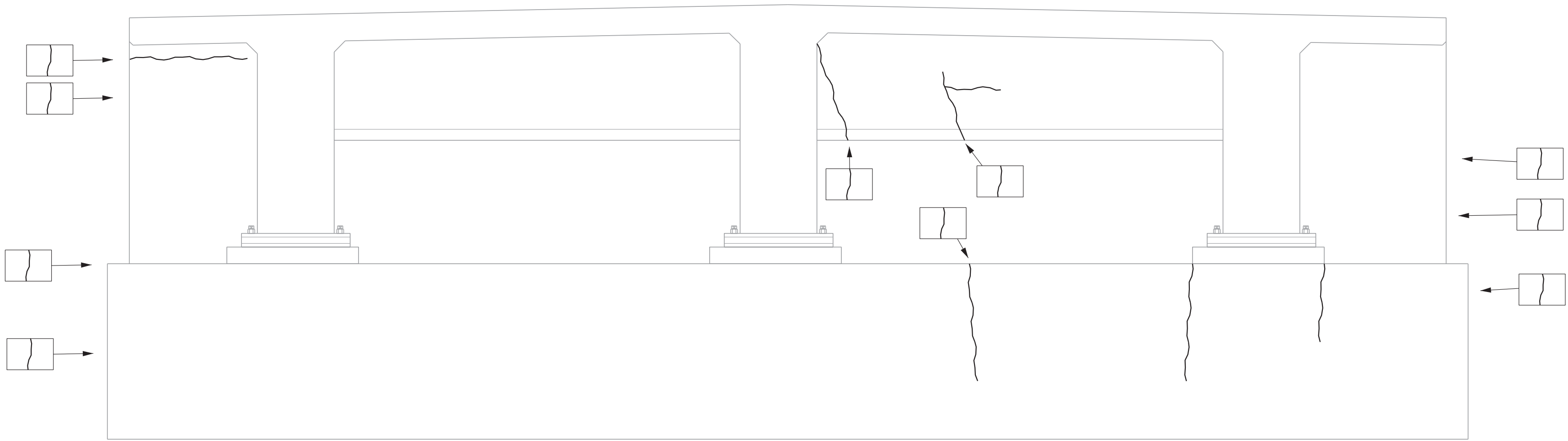
DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05, CONCRETE AND/OR 604-10.54, CONCRETE REPAIRS. SEE CONCRETE REPAIR DETAILS ON DWG. NO. BR-133-278.

PROJECT NO.		YEAR	SHEET NO.
82I026-M3-003		2025	B-06
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS.



ABUTMENT 1 ELEVATION
LOOKING BACK



ABUTMENT 2 ELEVATION
LOOKING AHEAD

ESTIMATED QUANTITIES		
LOCATION OF REPAIR	APPROX. REPAIR AREAS ITEM NO. 604-10.05 AND/OR 604-10.54, CONCRETE/CONCRETE REPAIRS (S.F.)	APPROX. REPAIR AREAS ITEM NO. 604-10.62, EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE) L.F.
ABUTMENT 1	2	-
ABUTMENT 2	-	30


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ABUTMENT REPAIRS

BRIDGE NO. 82-0B357-0.05
FEDERAL BRIDGE ID NO.
82I00260013
WESTFIELD DRIVE (B357)
OVER I-26, LM 0.05
SULLIVAN COUNTY
2025

BR-133-273

SEALED BY



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PIN NO.:

134991.00

DESIGN BY:

SUSANNE DAWSON

DATE:

MARCH 2024

DRAWN BY:

D. PICKEL

DATE:

MARCH 2024

SUPERVISED BY:

SUSANNE DAWSON

DATE:

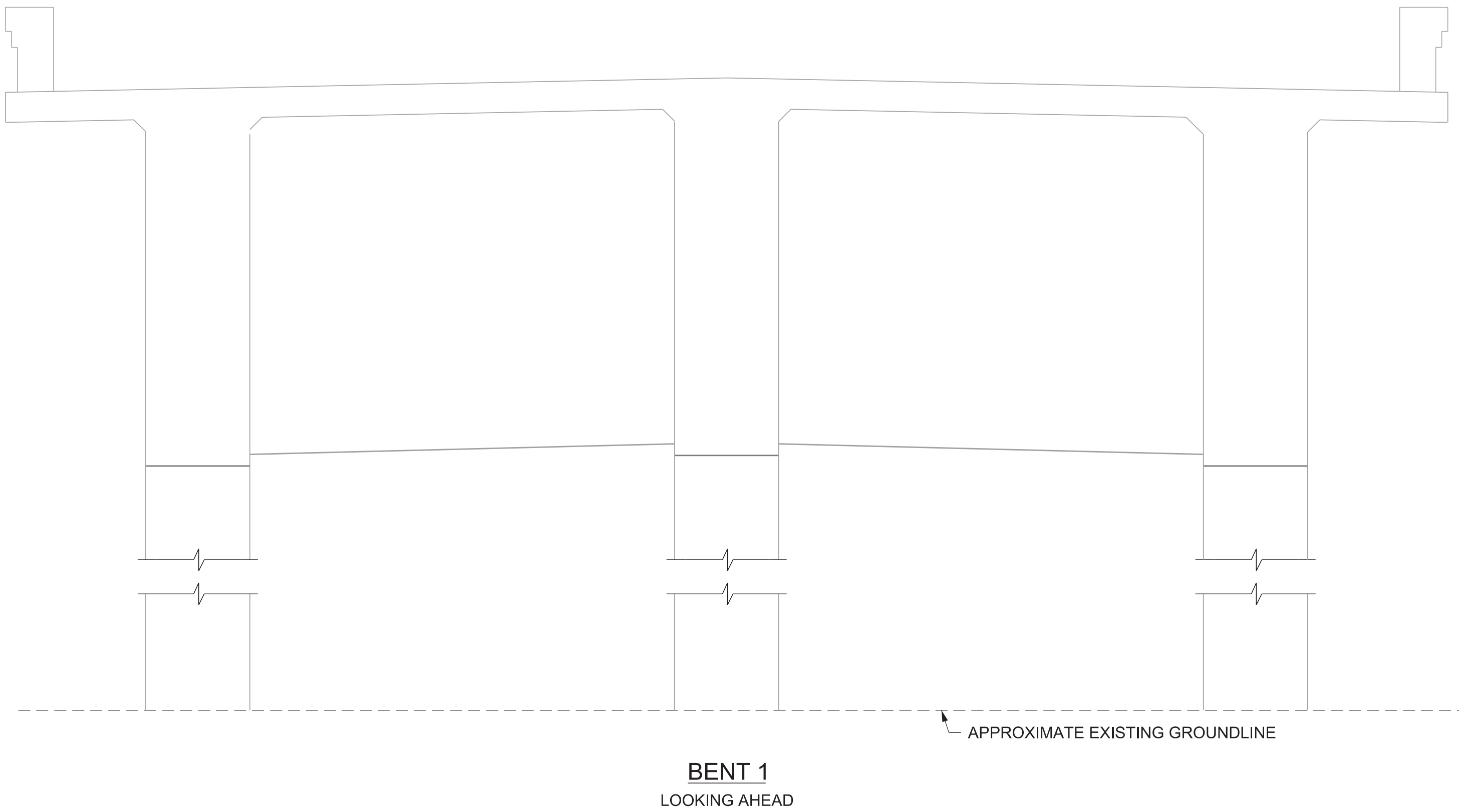
MARCH 2024

CHECKED BY:

FRANK BALE


DATE:


MARCH 2024



PROJECT NO.		YEAR	SHEET NO.
82I026-M3-003		2025	B-07
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS.

- 

DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 AND/OR 604-10.54, CONCRETE/CONCRETE REPAIRS. SEE CONCRETE REPAIR DETAILS ON DWG. NO. BR-133-278.
- 

DENOTES CRACK TO BE EPOXY INJECTED. QUANTITIES AND LOCATIONS SHOWN ARE APPROXIMATE AND MAY BE INCREASED, DECREASED OR ELIMINATED BY THE ENGINEER. SEE EPOXY INJECTION NOTES ON DWG. NO. BR-133-278.

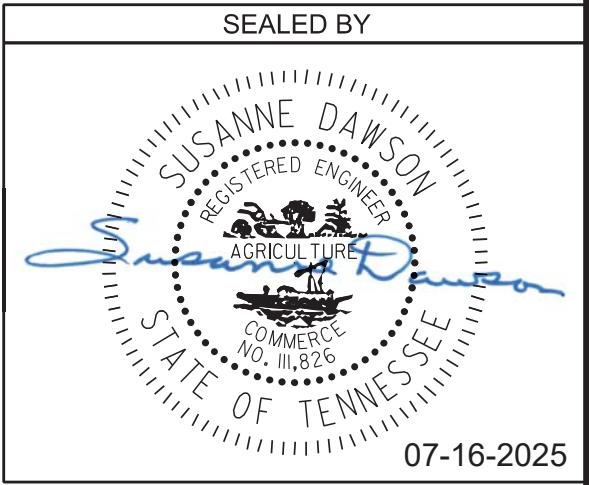
ESTIMATED QUANTITIES		
LOCATION OF REPAIR	APPROX. REPAIR AREAS ITEM NO. 604-10.05 AND/OR 604-10.54, CONCRETE/CONCRETE REPAIRS (S.F.)	APPROX. REPAIR AREAS ITEM NO. 604-10.62, EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE) (L.F.)
BENT 1	1	17

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BENT 1 REPAIRS

BRIDGE NO. 82-0B357-0.05
FEDERAL BRIDGE ID NO.
82I00260013
WESTFIELD DRIVE (B357)
OVER I-26, LM 0.05
SULLIVAN COUNTY
2025

BR-133-274



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PIN NO.:

134991.00

DESIGN BY:

SUSANNE DAWSON

DATE:

MARCH 2024

DRAWN BY:

D. PICKEL

DATE:

MARCH 2024

SUPERVISED BY:

SUSANNE DAWSON

DATE:

MARCH 2024

CHECKED BY:

FRANK BALE

DATE:


MARCH 2024




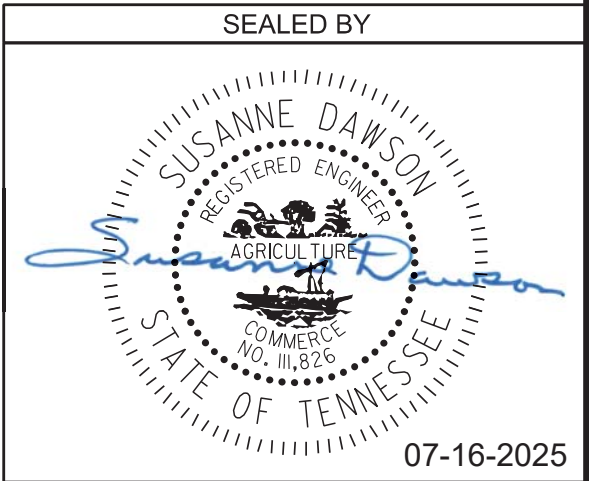
ESTIMATED QUANTITIES		
LOCATION OF REPAIR	APPROX. REPAIR AREAS ITEM NO. 604-10.05 AND/OR 604-10.54, CONCRETE/CONCRETE REPAIRS (S.F.)	APPROX. REPAIR AREAS ITEM NO. 604-10.62, EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE) (L.F.)
BENT 2	2	26

PROJECT NO.		YEAR	SHEET NO.
82I026-M3-003		2025	B-08
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS.

 DENOTES AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 AND/OR 604-10.54, CONCRETE/CONCRETE REPAIRS. SEE CONCRETE REPAIR DETAILS ON DWG. NO. BR-133-278.

 DENOTES CRACK TO BE EPOXY INJECTED. QUANTITIES AND LOCATIONS SHOWN ARE APPROXIMATE AND MAY BE INCREASED, DECREASED OR ELIMINATED BY THE ENGINEER. SEE EPOXY INJECTION NOTES ON DWG. NO. BR-133-278.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BENT 2 REPAIRS

BRIDGE NO. 82-0B357-0.05
FEDERAL BRIDGE ID NO.
82I00260013
WESTFIELD DRIVE (B357)
OVER I-26, LM 0.05
SULLIVAN COUNTY
2025

BR-133-275

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PIN NO.:

134991.00

DESIGN BY:

SUSANNE DAWSON

DATE: MARCH 2024

DRAWN BY:

D. PICKEL

DATE: MARCH 2024

SUPERVISED BY:

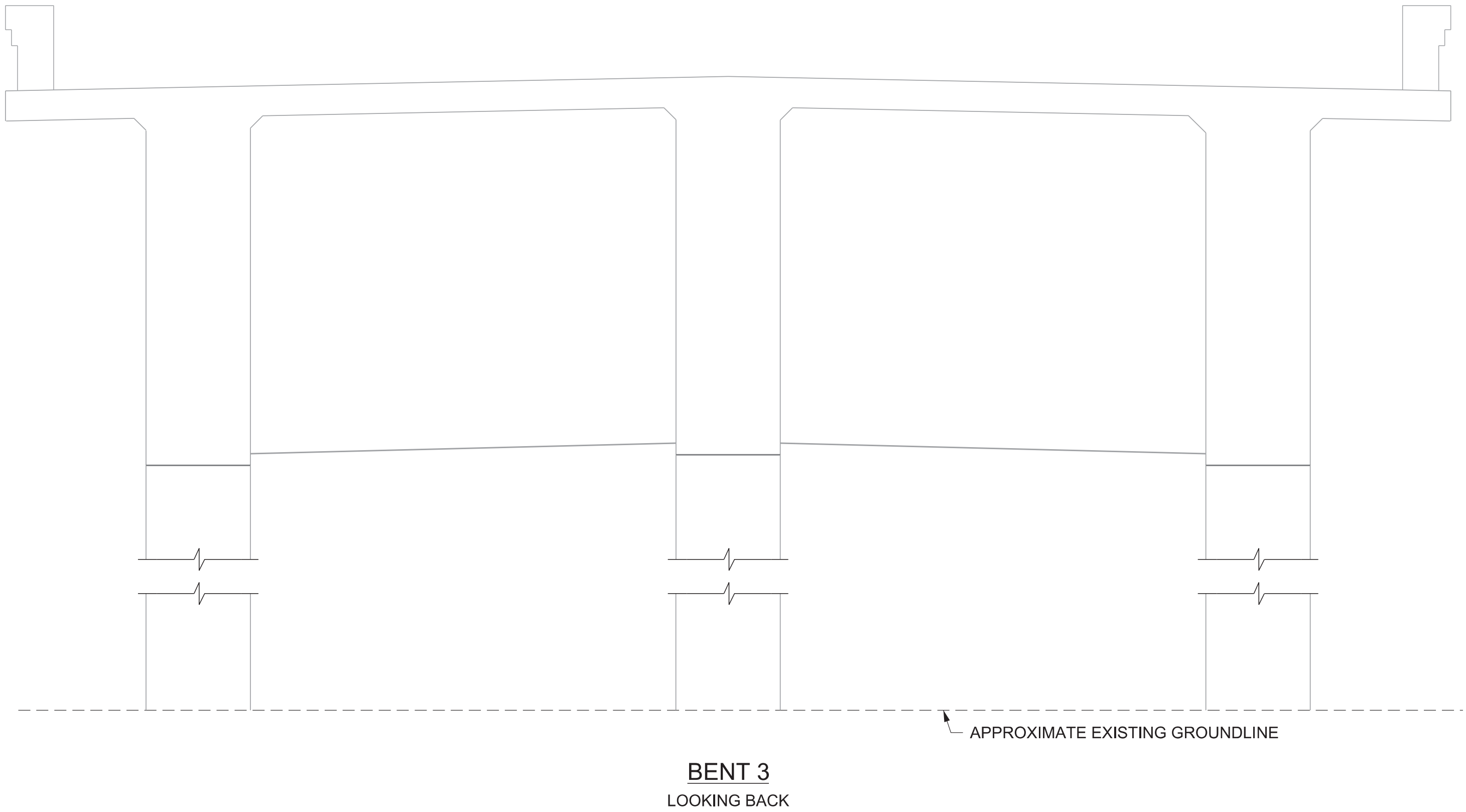
SUSANNE DAWSON

DATE: MARCH 2024

CHECKED BY:

FRANK BALE

DATE: MARCH 2024



PROJECT NO.		YEAR	SHEET NO.
82I026-M3-003		2025	B-09
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE STABILITY OF THE STRUCTURE DURING REPAIRS.

DENOTES CRACK TO BE EPOXY INJECTED. QUANTITIES AND LOCATIONS SHOWN ARE APPROXIMATE AND MAY BE INCREASED, DECREASED OR ELIMINATED BY THE ENGINEER. SEE EPOXY INJECTION NOTES ON DWG. NO. BR-133-278.

SEALED BY

07-16-2025

ESTIMATED QUANTITIES	
LOCATION OF REPAIR	APPROX. REPAIR AREAS ITEM NO. 604-10.62, EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE) (L.F.)
BENT 3	16

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BENT 3 REPAIRS

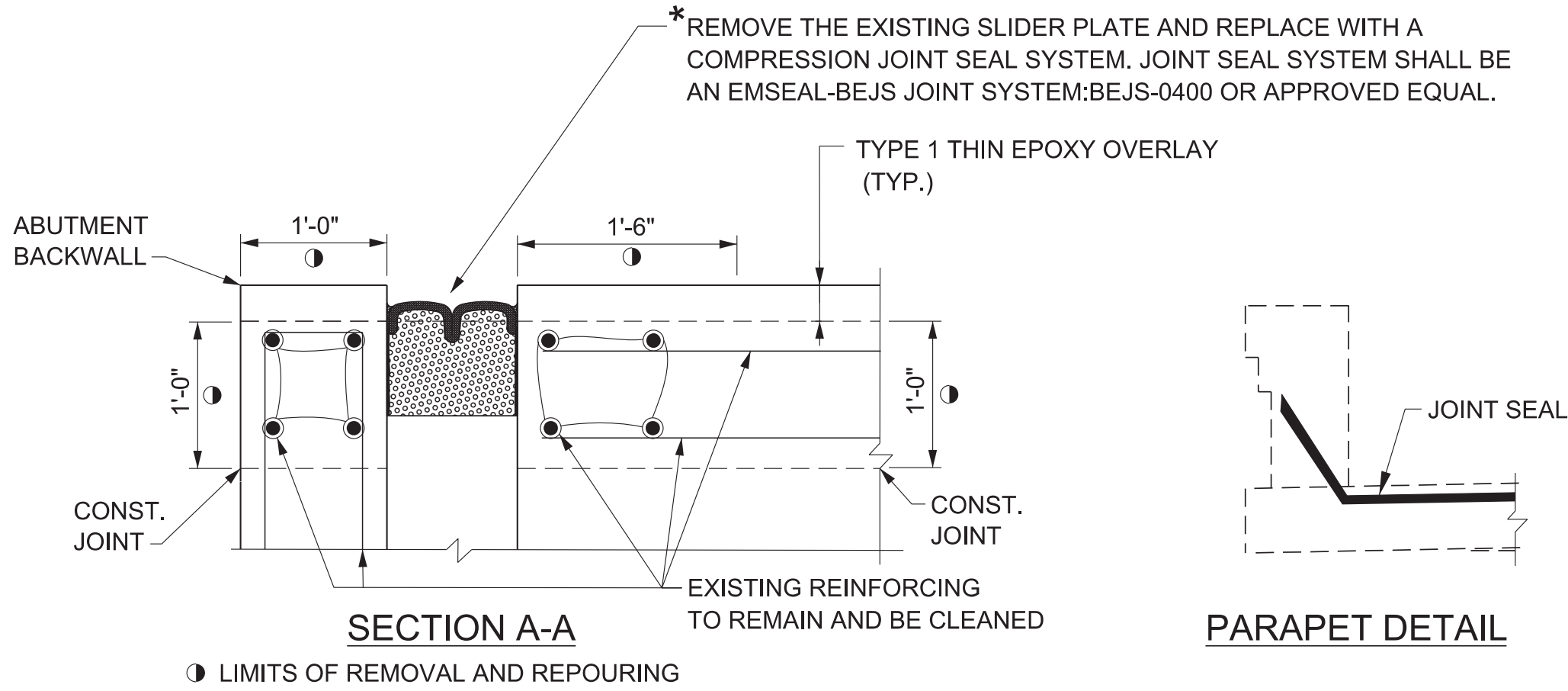
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FEDERAL BRIDGE ID NO.
82I00260013
WESTFIELD DRIVE (B357)
OVER I-26, LM 0.05
SULLIVAN COUNTY
2025

BR-133-276

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PIN NO.:	134991.00	
DESIGN BY:	SUSANNE DAWSON	DATE: MARCH 2024
DRAWN BY:	D. PICKEL	DATE: MARCH 2024
SUPERVISED BY:	SUSANNE DAWSON	DATE: MARCH 2024
CHECKED BY:	FRANK BALE	DATE: MARCH 2024

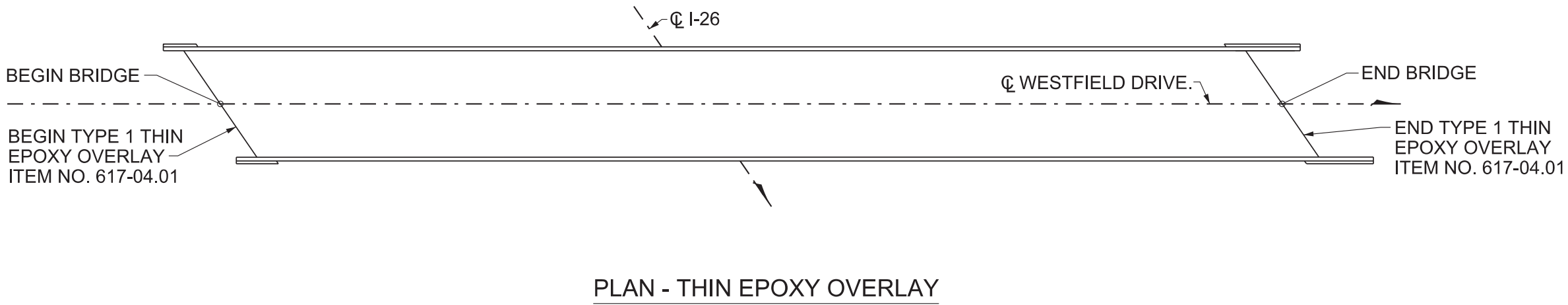


EXPANSION JOINT REPLACEMENT DETAILS

* THE JOINT SYSTEM SHALL BE INSTALLED UNDER THE DIRECT SUPERVISION OF AN AUTHORIZED TECHNICIAN PROVIDED BY THE EXPANSION JOINT SUPPLIER FOR EACH JOINT IN THE BRIDGE. THE TECHNICIAN MUST APPROVE ALL ASPECTS OF THE GEOMETRY AND PREPARATION, INCLUDING GRINDING AND/OR GROOVING, PRIOR TO ANY JOINT MATERIAL INSTALLATION. THE JOINT SEAL MUST BE CONTINUOUS WITH NO FIELD SPLICES AND NO FIELD CUTTING.

COST OF REMOVAL OF THE EXISTING CONCRETE, PLATES, ANGLES, ANCHORS, CLEANING AND MAINTAINING THE EXISTING REINFORCING IN THE EXPANSION JOINT REPAIR AREAS, HIGH EARLY STRENGTH CONCRETE, FORMING, AND ALL MISCELLANEOUS MATERIALS AND LABOR NECESSARY TO INSTALL THE NEW COMPRESSION JOINT SEAL SYSTEM COMPLETE AND IN PLACE TO BE PAID FOR IN THE PRICE BID FOR ITEM NO. 604-10.44, EXPANSION JOINT REPAIRS.

TABLE OF JOINT WIDTHS		
POURING TEMP.	EXPANSION JOINT WIDTH	
	ABUTMENT 1	ABUTMENT 2
40° F	2 ¹ / ₁₆ "	2 ¹ / ₁₆ "
50° F	2 ¹ / ₈ "	2 ¹ / ₈ "
60° F	2 ⁷ / ₈ "	2 ⁷ / ₈ "
70° F	3"	3"
80° F	3 ¹ / ₈ "	3 ¹ / ₈ "
90° F	3 ³ / ₁₆ "	3 ³ / ₁₆ "



THIN EPOXY OVERLAY NOTES

TYPE 1 THIN EPOXY OVERLAY SYSTEM: USE DECK PRETREATMENT/PRIMER PER MANUFACTURER'S RECOMMENDATION AND 2 LIFTS OF AN EPOXY-URETHANE COPOLYMER AND AGGREGATE. TYPE 1 OVERLAY SHALL BE APPLIED MECHANICALLY USING METERED EQUIPMENT; HAND MIXING OF MATERIAL IS NOT PERMITTED. THIN OVERLAY SYSTEM SHALL BE FROM THE QUALIFIED PRODUCTS LIST 23.005 TYPE 1 THIN OVERLAY (EPOXY URETHANE). MINIMUM OVERLAY THICKNESS SHALL BE 3/8 INCH.

APPLICATION EQUIPMENT SHOULD :

- A) BE CAPABLE OF METERING, MIXING AND DISTRIBUTING THE POLYMER AND PRETREATMENT TO MANUFACTURER'S RECOMMENDATION.
- B) USE AN APPLICATION MACHINE THAT FEATURES POSITIVE DISPLACEMENT VOLUMETRIC METERING PUMPS CONTROLLED BY A HYDRAULIC POWER UNIT.
- C) STORE COMPONENTS IN TEMPERATURE CONTROLLED RESERVOIRS CAPABLE OF MAINTAINING 100 DEGREES FAHRENHEIT (PLUS OR MINUS 10 DEGREES) TO INSURE OPTIMAL MIXING.
- D) CHECK MIXING RATIO AT THE PUMP OUTLETS AS WELL AS CYCLE COUNTING CAPABILITIES TO MONITOR OUTPUT ON STANDARD FEATURES.
- E) USE MOTIONLESS IN-LINE MIXING SO AS TO NOT OVERLY SHEAR THE MATERIAL TO ENTRAP AIR IN THE MIX.
- F) MAXIMIZE MATERIAL WORKING TIME BY MIXING IT IMMEDIATELY BEFORE DISPENSING.

AGGREGATE SHALL BE ANGULAR, HAVING LESS THAN 0.2% MOISTURE AND FREE OF DIRT, CLAY, ASPHALT AND OTHER FOREIGN OR ORGANIC MATERIALS. AGGREGATE FOR ALL LAYERS SHALL BE BAUXITE OR FLINT ROCK PRODUCTS FLINT MEETING THE FOLLOWING GRADATION:

SIEVE SIZE	% PASSING
NO. 6	95-100
NO. 10	10-35
NO. 20	0-3

FULL AND PARTIAL DEPTH DECK REPAIR SHALL CURE A MINIMUM OF 28 DAYS BEFORE THE OVERLAY IS PLACED. THE 28 DAYS MAY BE WAIVED IF THE OVERLAY MANUFACTURER PROVIDES A METHOD OF TESTING THE REPAIRED AREAS AND APPROVES THE PLACEMENT BY LETTER. TRAFFIC SHALL BE ALLOWED TO USE THE BRIDGE DURING THE CURING PERIOD OF THE PATCHES BUT NOT AFTER SHOTBLASTING. MAGNESIUM PHOSPHATE BASED MATERIALS WILL NOT BE ALLOWED.

THE CONCRETE DECK SURFACE SHALL BE CLEANED BY SHOTBLASTING TO REMOVE ANY OIL, DIRT, RUBBER, TRAFFIC STRIPING, OR ANY OTHER POTENTIAL DETRIMENTAL MATERIAL SUCH AS CURING COMPOUND AND LAITANCES, WHICH THE MANUFACTURER AND ENGINEER'S OPINION WOULD PREVENT PROPER BONDING AND CURING OF THE MATERIAL. IN AREAS WHERE SHOTBLASTING EQUIPMENT CAN NOT REACH (I.E., ALONG CURBS AND BRIDGE RAILS) SANDBLASTING IS PERMITTED TO AN EXTENT TO THE ENGINEER'S AND MANUFACTURER'S APPROVAL. IMMEDIATELY BEFORE APPLICATION, ALL PREPARED SURFACES SHALL BE CLEANED WITH COMPRESSED AIR OR VACUUMED TO REMOVE DUST AND DEBRIS. THE CONTRACTOR IS TO PREVENT THE TRACKING OF TACK COAT AND CONSTRUCTION DEBRIS ACROSS THE BRIDGE DECK PRIOR TO APPLICATION OF THE THIN EPOXY OVERLAY. MILLING THE BRIDGE DECK WILL NOT BE AN OPTION FOR TACK COAT OR DEBRIS REMOVAL. REMOVAL SHALL BE AT THE CONTRACTOR'S EXPENSE.

ALL SURFACES THAT ARE TREATED SHALL BE DRY AT THE TIME OF APPLICATION. THE OVERLAY SHALL NOT BE APPLIED WHEN IT HAS RAINED 24 HOURS PRIOR TO, OR RAIN IS FORECAST WITHIN 8 HOURS AFTER, APPLICATION. THE MOISTURE CONTENT IN THE DECK SUBSTRATE SHALL BE TESTED. MOISTURE IS NOT TO EXCEED 4.5 PERCENT WHEN MEASURED BY ELECTRONIC METER. IF THE TEST SHOWS EXCESS MOISTURE, THE DECK SHALL CONTINUE TO DRY BEFORE APPLICATION PROCEEDS.

BLUSHING (A WAXY SURFACE COATING ON THE EPOXY) IS CAUSED BY THE REACTION OF MOISTURE WITH THE HARDENING AGENT. BLUSHING CREATES A SURFACE THAT MAKES FUTURE LAYERS DIFFICULT TO ADHERE. LIFTS THAT SHOW SIGNS OF BLUSHING SHALL BE REMOVED AND REPLACED PRIOR TO APPLICATION OF THE NEXT. THE COST TO REMOVE AND REPLACE THESE AREAS SHALL BE AT THE CONTRACTOR'S EXPENSE.

TRAFFIC, OTHER THAN APPLICATION EQUIPMENT, SHALL NOT BE ALLOWED ON ANY PORTION OF THE DECK THAT HAS BEEN SHOTBLASTED OR WHERE PART OF THE APPLICATION HAS BEEN PLACED.

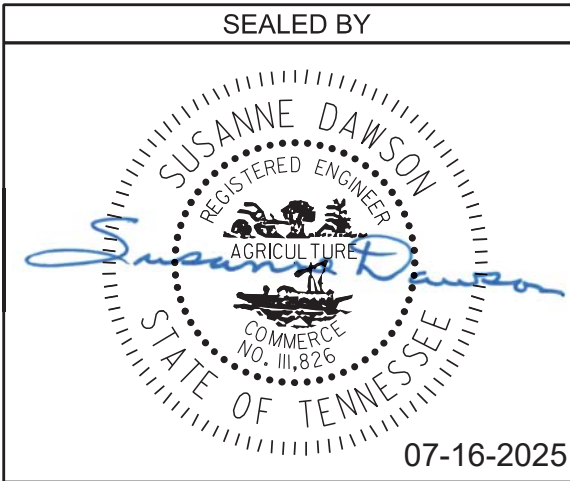
SEE MANUFACTURER'S RECOMMENDATIONS FOR REQUIRED AMBIENT AND SURFACE TEMPERATURES AND HUMIDITY LIMITS FOR APPLICATION.

THE MANUFACTURER SHALL HAVE A REPRESENTATIVE ON THE JOB SITE AT ALL TIMES DURING APPLICATION AND CURE TIME. THE REPRESENTATIVE, ALONG WITH CONSULTATION WITH ENGINEER, MAY SUSPEND ANY ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECIFICATIONS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REPRESENTATIVE ARE SATISFIED THAT APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

ALL COSTS FOR AGGREGATE, EPOXY FOR MINIMUM OF TWO LIFTS, SURFACE PREPARATION, LABOR AND ANY OTHER MISCELLANEOUS MATERIALS REQUIRED TO PLACE THIN OVERLAY SHALL BE INCLUDED IN ITEM NO. 617-04.01, TYPE 1 THIN EPOXY OVERLAY (EPOXY URETHANE), SY.

THICKNESS VERIFICATION: THE PROJECT ENGINEER SHALL BE NOTIFIED OF THE NUMBER OF GALLONS USED ON THE PROJECT WITH NOTARIZED QUANTITY STATEMENTS FROM THE CONTRACTOR AND THE MANUFACTURER. THE CONTRACTOR SHALL VERIFY TO TDOT THAT THE OVERLAY IS AN AVERAGE OF AT LEAST 3/8 INCH THICK AT THREE RANDOM LOCATIONS AGREED UPON BY THE PROJECT ENGINEER AND THE MATERIAL MANUFACTURER REPRESENTATIVE. IF 3/8 INCH AVERAGE IN NOT ACHIEVED, A RETEST SHALL BE PERFORMED IN ADJOINING AREAS. THIN AREAS SHALL BE RE-COATED AS DESCRIBED ABOVE BY THE CONTRACTOR AND RE-VERIFIED AT NO ADDITIONAL COST TO TDOT. THIS VERIFICATION MAY CONSIST OF CORES, HOLES, ETC., BUT IN ALL CASES, ANY DESTRUCTIVELY TESTED AREAS SHALL BE REPAIRED BY THE CONTRACTOR BEFORE FINAL ACCEPTANCE BY THE PROJECT ENGINEER.

PROJECT NO.		YEAR	SHEET NO.
82I026-M3-003		2025	B-10
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION JOINT REPLACEMENT AND EPOXY OVERLAY DETAILS

BRIDGE NO. 82-0B357-0.05
FEDERAL BRIDGE ID NO.
82I00260013
WESTFIELD DRIVE (B357)
OVER I-26, LM 0.05
SULLIVAN COUNTY
2025

BR-133-277

SPECIAL NOTES FOR EPOXY INJECTION

UNLESS OTHERWISE NOTED, THE INTENT OF THIS SPECIFICATION IS FOR DESIGNATED CRACKS TO BE INJECTED THEIR FULL LENGTH AND DEPTH.

DESIGNATED CRACKS SHALL BE INJECTED WITH AN APPROVED EPOXY RESIN ADHESIVE FILLING ALL VOIDS FOR THE CRACK DEPTH OR THICKNESS OF THE MEMBER. THE EPOXY RESIN ADHESIVE SHALL BE ON THE CURRENT QUALIFIED PRODUCTS LIST MAINTAINED BY THE DIVISION OF MATERIALS AND TEST. ALL CRACKS SHALL BE INJECTED USING AN ADHESIVE SUITABLE FOR THE FIELD CONDITIONS (CRACK WIDTH, TEMPERATURE, HUMIDITY, ETC.) RECOMMENDED BY THE ADHESIVE MANUFACTURER AS SHOWN ON MATERIAL DATA SHEETS. FOLLOWING INJECTION, ALL INJECTION PORTS AND CAPPING MATERIAL SHALL BE REMOVED FROM EXPOSED SURFACES LEAVING THE SURFACE SMOOTH AND FLUSH WITH THE SURROUNDING CONCRETE SURFACES.

THE CONTRACTOR SHALL HAVE SUFFICIENT EXPERIENCE AND TRAINING TO PERFORM THE EPOXY INJECTION IN ACCORDANCE WITH THESE PLANS. PRIOR TO PERFORMING ANY WORK, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER A WRITTEN PROCEDURE FOR PERFORMING THE EPOXY INJECTION. THE PROCEDURE SHALL DESCRIBE IN DETAIL HOW THE WORK WILL BE PERFORMED. THE PROCEDURE SHALL INCLUDE, BUT NOT BE LIMITED TO THE FOLLOWING INFORMATION.

- 1) DESCRIPTION OF EQUIPMENT.

A. THE INJECTION EQUIPMENT SHALL BE OF THE TYPE THAT MIXES ADHESIVE COMPONENTS AT THE INJECTION HEAD.

B. THE INJECTION EQUIPMENT SHALL BE CAPABLE OF DISCHARGING MIXED ADHESIVE AT ANY PRESSURE UP TO 300 PSI. THE INJECTION EQUIPMENT SHALL BE EQUIPPED WITH GAUGES WHICH CAN MEASURE THE INJECTION PRESSURE AND VOLUME.
- 2) EQUIPMENT CALIBRATION PROCEDURES AND SCHEDULE.
- 3) MATERIALS TO BE USED (INCLUDING MANUFACTURER DATA SHEETS)

A. CAPPING MATERIAL

B. EPOXY ADHESIVE (TYPE TO BE APPROPRIATE FOR CRACK SIZES TO BE INJECTED).
- 4) PORT SPACING

A. PORT SPACING SHALL NOT BE LESS THAN THE THICKNESS OF THE CONCRETE IN THAT LOCATION.
- 5) INJECTION SEQUENCE

A. INJECTION SHALL PROCEED FROM LOWER END OF CRACK ALONG ADJACENT PARTS.

B. SKIPPING OF PORTS DURING INJECTION SHALL NOT BE ALLOWED.

THE CONTRACTOR SHALL HAVE THE MANUFACTURER'S INSTRUCTIONS FOR PROPORTIONING AND MIXING AVAILABLE AT THE JOB SITE AT ALL TIMES AND SHALL ENSURE THAT THE EQUIPMENT IS SUPPLYING THE MIXED ADHESIVE IN THE CORRECT PROPORTIONS.

TO ENSURE PROPER MIXING AND PROPORTIONING, SAMPLES SHALL BE TAKEN FROM THE INJECTOR HEAD AT THE START OF EACH WORKDAY AND EACH TIME THE ADHESIVE RESERVOIRS ARE REFILLED. THE SAMPLES SHALL BE IN A TEST CUP. THE SAMPLE SHALL BE MONITORED TO ENSURE THAT THE CURE TIME IS IN COMPLIANCE WITH THE MANUFACTURER'S DATA SHEETS. IF THE SAMPLES DO NOT CURE IN THE SPECIFIED TIME THEN THE EQUIPMENT USED TO PRODUCE THE SAMPLE SHALL NOT BE USED UNTIL THE PROBLEM IS CORRECTED.

CORE SAMPLES SHALL BE TAKEN AS VERIFICATION OF THE QUALITY OF WORK. THE CONTRACTOR SHALL TAKE ONE (1) ONE (1) INCH DIAMETER (FULL DEPTH OF CONCRETE AT LOCATION CORED) CORE SAMPLES STARTING WITH THE FIRST REPAIR LOCATION THEN EVERY THIRD REPAIR LOCATION AFTERWARDS. WORK SHALL NOT PROCEED UNTIL THE CORE SAMPLE IS TAKEN AND ACCEPTED. ALL CORE SAMPLES AND HOLES SHALL BE INDEXED FOR FUTURE REFERENCE. THE ENGINEER SHALL DESIGNATE ALL LOCATIONS TO BE CORED. IF ANY CORES SHOW UNACCEPTABLE RESULTS, ALL WORK SHALL BE STOPPED UNTIL THE CONTRACTOR SUBMITS A PROPOSAL FOR CORRECTING UNACCEPTABLE WORK.

THE INITIAL CORE WILL ALSO SERVE TO QUALIFY THE FOREMAN FOR THIS WORK. IF AT ANY TIME A NEW FOREMAN IS USED, HE SHALL BE QUALIFIED WITH A CORE SAMPLE.

THE CONTRACTOR, AT HIS EXPENSE, SHALL REPAIR ALL CORE HOLES WITH AN APPROVED CEMENTITIOUS PATCHING MATERIAL.

CORE SAMPLES SHALL BE VISUALLY INSPECTED TO CONFIRM THAT CRACKS ARE COMPLETELY FILLED WITH ADHESIVE. ANY CORE HAVING LESS THAN 95% OF THEM CRACK FILLED WITH ADHESIVE SHALL BE CONSIDERED UNACCEPTABLE AND BE REJECTED.

CORE SAMPLES SHALL BE TESTED FOR BOND STRENGTH. SAMPLES MAY BE FRACTURED BY HAMMER BLOW TO CRACK AREA OR THROWN AT A HARD SURFACE. IF ADHESIVE FAILURE OCCURS BEFORE CONCRETE FAILURE, THE CORE SHALL BE CONSIDERED UNACCEPTABLE AND REJECTED.

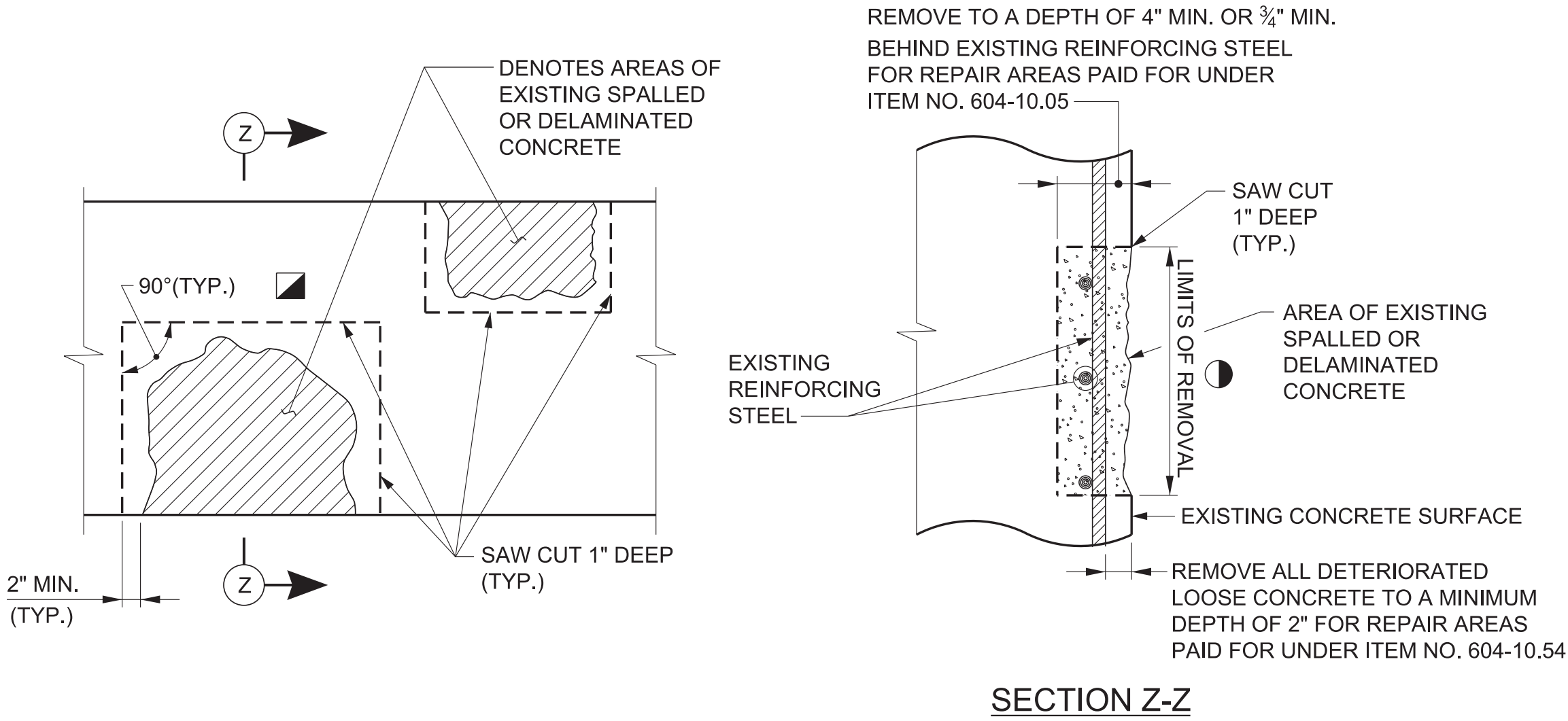
PAYMENT FOR EPOXY INJECTION CRACK REPAIR SHALL BE MADE UNDER ITEM NUMBERS 604-10.62, EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE), L.F. AND 604-10.58, EPOXY INJECTION (INJECTION), GAL.

PRICE BID FOR ITEM NUMBER 604-10.62, EPOXY INJECTION REPAIR (COMPLETE AND IN PLACE), L.F. SHALL INCLUDE COST OF ALL LABOR AND MATERIALS (EXCEPT ADHESIVE) FOR GRINDING FOR SURFACE PREPARATION, CRACK PREPARATION, CAPPING, INJECTION OF ADHESIVE, ALL SAMPLES AND TESTING, REMOVAL OF CAPPING MATERIAL AND PORTS. AND OTHER INCIDENTALS. CRACKS SHALL BE MEASURED FOR PAYMENT ALONG THE LENGTH OF THE VISIBLE SURFACE CRACK.

PRICE BID FOR ITEM 604-10.58, EPOXY INJECTION (INJECTION), GAL., SHALL INCLUDE COST FOR ADHESIVE MATERIAL INJECTED ONLY.

NO PAYMENT SHALL BE MADE FOR REWORK DEEMED NECESSARY BY FAILURE OF ADHESIVE SAMPLES OR CORE SAMPLES.

ALL WORK INCLUDING SAMPLING AND TESTING SHALL BE IN THE PRESENCE OF THE ENGINEER OR HIS REPRESENTATIVE OR CONTRACT INSPECTORS. ANY WORK DONE WITHOUT INSPECTORS PRESENT SHALL NOT BE PAID FOR. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH WEEKLY SCHEDULES OF WORK TO BE PERFORMED. SCHEDULES SHALL BE SUBMITTED AT LEAST THREE (3) DAYS IN ADVANCE OF WORK TO BE DONE. THE ENGINEER SHALL BE NOTIFIED OF ANY CHANGE IN THE SCHEDULE A MINIMUM OF TWENTY-FOUR (24) HOURS IN ADVANCE OF CHANGE.



DETAILS SHOWING AREAS OF EXISTING SPALLED OR DELAMINATED CONCRETE SURFACES TO BE REMOVED AND REPAIRED

- DENOTES LIMITS AND LOCATION OF REPAIRS TO BE DESIGNATED BY THE ENGINEER
- DENOTES SAW CUT EXISTING CONCRETE SURFACES SO AS TO OBTAIN SQUARED CORNERS

NOTES

EXTREME CARE SHALL BE TAKEN WHEN REMOVING THE EXISTING SPALLED OR DELAMINATED CONCRETE SO AS NOT TO DAMAGE THE EXISTING REINFORCING STEEL. ALL EXPOSED EXISTING REINFORCING STEEL SHALL RECEIVE A COMPLETE CLEANING TO REMOVE ALL RUST. ALL EXISTING REINFORCEMENT SHALL REMAIN IN PLACE AND INCORPORATED INTO THE NEW CONSTRUCTION. ALL WORK MUST MEET WITH THE FULL APPROVAL OF THE ENGINEER.

THE ENGINEER SHALL HAVE THE OPTION OF DESIGNATING A SPALLED OR DELAMINATED AREA TO BE REPAIRED UNDER ITEM NO. 604-10.05 OR 604-10.54. PATCHING MATERIAL FOR ITEM NO. 604-10.05 SHALL BE HIGH EARLY STRENGTH CONCRETE. PATCHING MATERIAL FOR ITEM NO. 604-10.54 SHALL BE A POLYMER MODIFIED CEMENTITIOUS STRUCTURAL PATCHING MATERIAL. SEE TDOT QUALIFIED PRODUCTS LIST 13, SECTION B, MATERIAL QPL 13.009.

PNEUMATICALLY PLACED CONCRETE IS NOT ALLOWED.

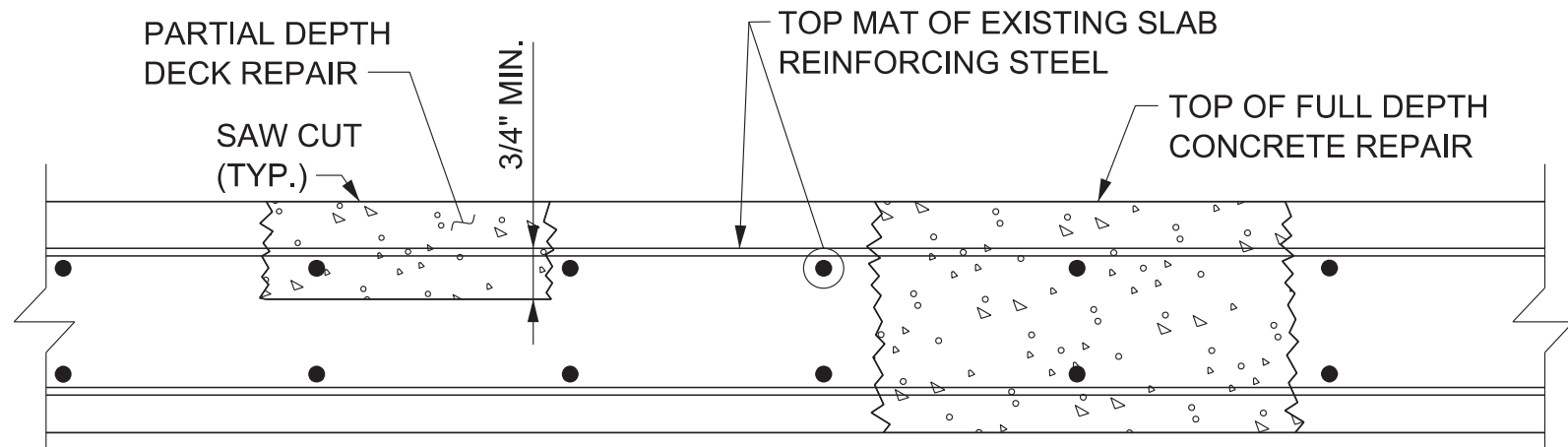
COST OF SAW CUTTING, REMOVING SPALLED OR DELAMINATED CONCRETE, CLEANING, PATCHING MATERIAL, LABOR AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS AS SHOWN TO BE INCLUDED IN ITEM NO. 604-10.54, CONCRETE REPAIRS, S.F. OR ITEM NO. 604-10.05, CONCRETE, S.F.

THE ENGINEER SHALL DESIGNATE ALL SPALLED OR DELAMINATED CONCRETE REPAIR AREAS IN THE FIELD. QUANTITIES GIVEN ARE APPROXIMATE. ITEM NO. 604-10.05 AND 604-10.54 MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.

POWER HAND DRIVEN TOOLS USED FOR REMOVAL OF UNSOUND CONCRETE ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:

1. PNEUMATIC HAMMERS HEAVIER THAN THE 35 LB. CLASS SHALL NOT BE USED.

2. CHIPPING HAMMERS OF THE 15 LB. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BEHIND REINFORCING STEEL.



DETAILS SHOWING FULL AND PARTIAL DEPTH DECK REPAIR

CONCRETE FOR FULL AND PARTIAL DEPTH DECK REPAIRS SHALL BE HIGH EARLY STRENGTH CONCRETE WITH A COMPRESSIVE STRENGTH OF 3,500 P.S.I. @ 28 DAYS. TRAFFIC SHALL NOT BE PERMITTED ON ANY REPAIR AREA UNTIL TEST SPECIMENS ATTAIN A COMPRESSIVE STRENGTH OF 3,000 P.S.I. MINIMUM AND THE CONCRETE HAS BEEN IN PLACE A MINIMUM OF 18 HOURS.

REMOVE CONCRETE IN ALL DELAMINATED AREAS TO A DEPTH OF 3/4" BELOW THE TOP BAR OF THE TOP MAT OF THE ORIGINAL SLAB REINFORCING STEEL. ALL REINFORCING STEEL IN AREAS OF DECK REPAIR SHALL BE COMPLETELY CLEANED. AREAS OF CONCRETE REMOVAL SHALL BE DESIGNATED BY PERSONNEL FROM THE BRIDGE REPAIR OFFICE. INSPECTIONS TO DETERMINE AREAS OF DECK REPAIR SHALL BE SCHEDULED WITH THE BRIDGE REPAIR OFFICE AT LEAST THREE (3) DAYS IN ADVANCE.

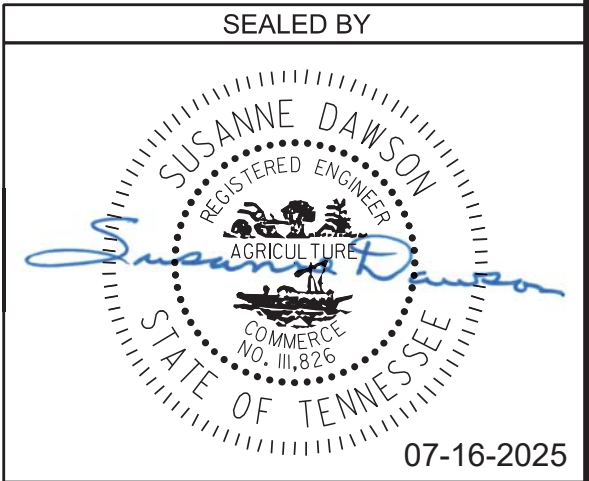
DECK REPAIR WILL BE PAID FOR UNDER ITEM NO. 604-10.50, BRIDGE DECK REPAIRS (PARTIAL DEPTH OF SLAB), AND 604-10.30, BRIDGE DECK REPAIRS (FULL DEPTH OF SLAB). DURING PARTIAL DEPTH REPAIRS, SHOULD DETERIORATED CONCRETE BE ENCOUNTERED WHICH APPEARS TO RUN FULL DEPTH IN THE SLAB, THE ENGINEER MAY DESIGNATE THESE AREAS TO BE REPAIRED UNDER ITEM NO. 604-10.30. POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE IN MAKING PARTIAL AND FULL DEPTH REPAIRS ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:

- 1) (PARTIAL DEPTH REPAIRS) PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 60 POUND CLASS SHALL NOT BE USED.
- 2) (FULL DEPTH REPAIRS) PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 90 POUND CLASS SHALL NOT BE USED.
- ALSO ALL DECK REPAIR OVER BEAMS WILL BE RESTRICTED TO 60 POUND PNEUMATIC HAMMERS.
- 3) CHIPPING HAMMERS OF THE 15 POUND CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.

ITEM NOS. 604-10.30 AND 604-10.50 MAY BE INCREASED, DECREASED, OR ELIMINATED AS DIRECTED BY THE ENGINEER.

EXTREME CARE SHALL BE TAKEN WHEN REMOVING DETERIORATED CONCRETE SO AS NOT TO DAMAGE THE EXISTING REINFORCING STEEL. ALL EXPOSED REINFORCING STEEL SHALL BE COMPLETELY CLEANED TO THE SATISFACTION OF THE ENGINEER BEFORE REPOURING.

PROJECT NO.		YEAR	SHEET NO.
82I026-M3-003		2025	B-11
REVISIONS			
NO.	DATE	BY	BRIEF DESCRIPTION



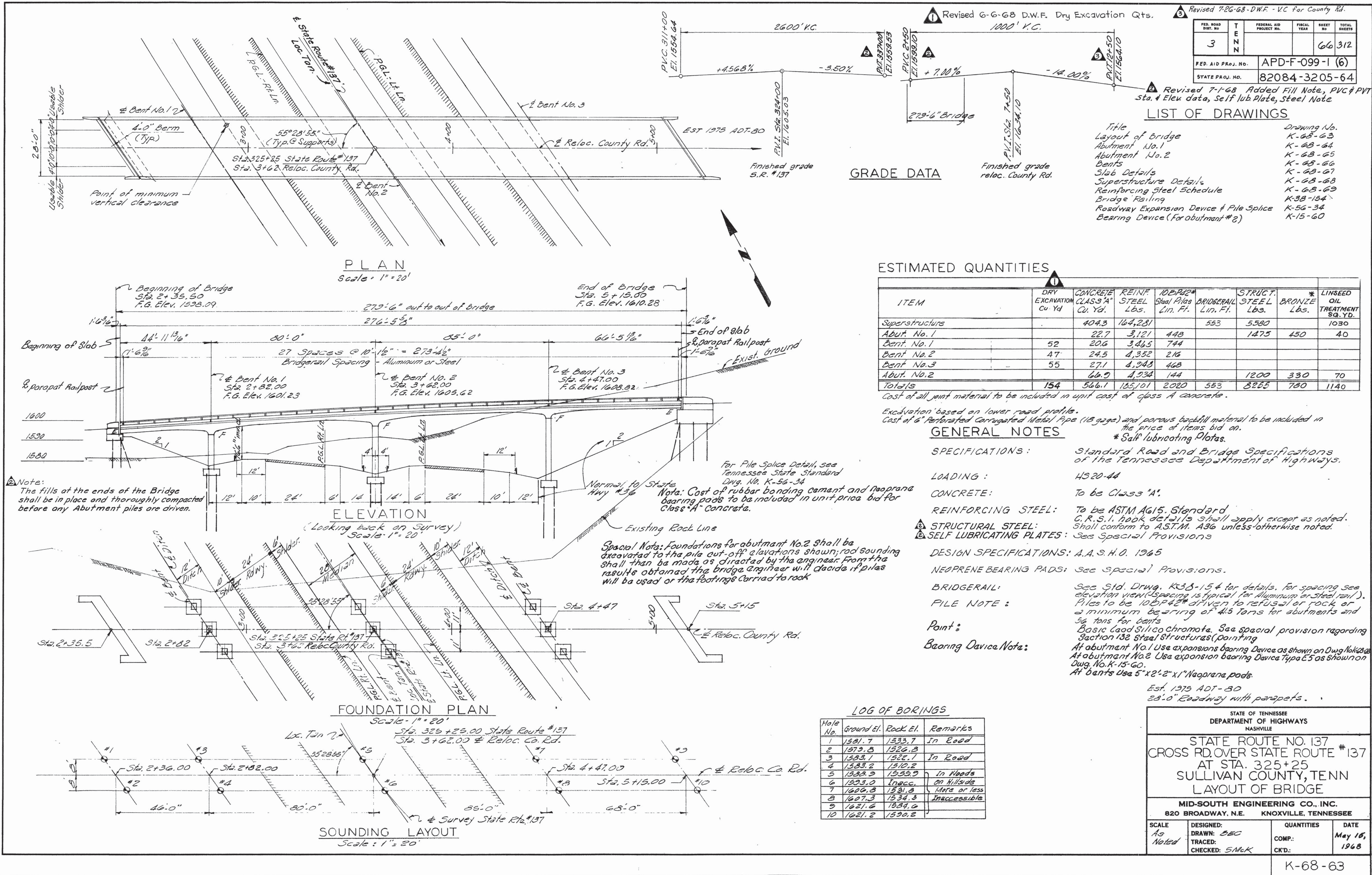
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

CONCRETE REPAIR DETAILS

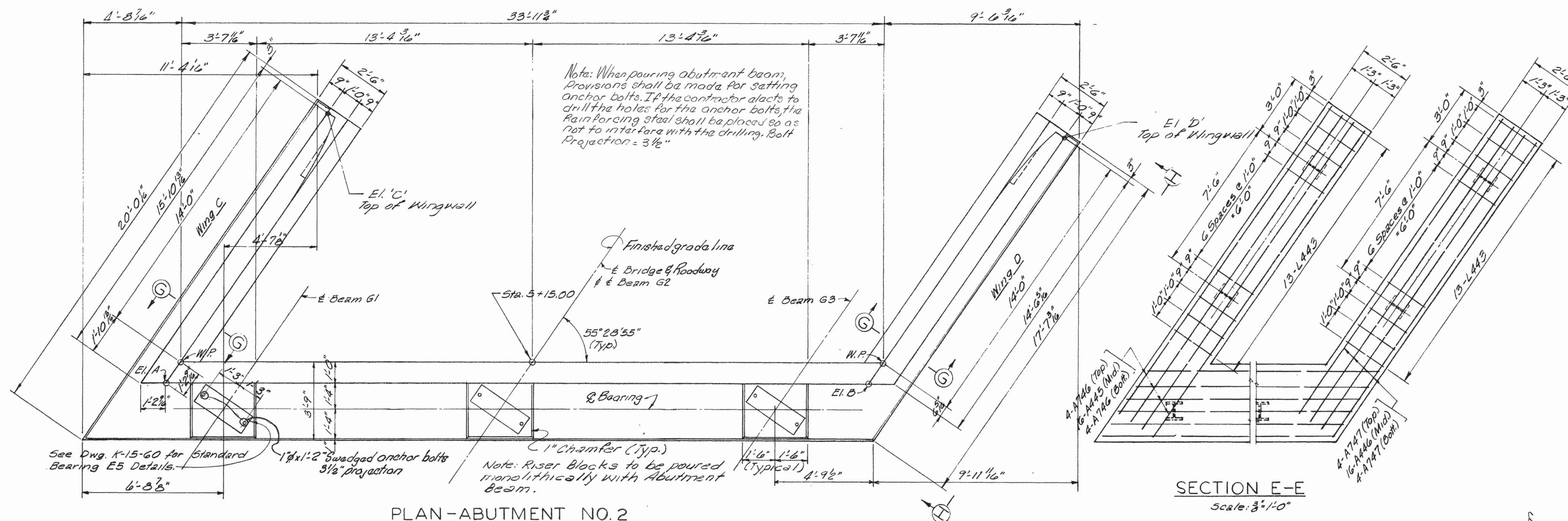
BRIDGE NO. 82-0B357-0.05
FEDERAL BRIDGE ID NO.
82I00260013
WESTFIELD DRIVE (B357)
OVER I-26, LM 0.05
SULLIVAN COUNTY
2025

BR-133-278

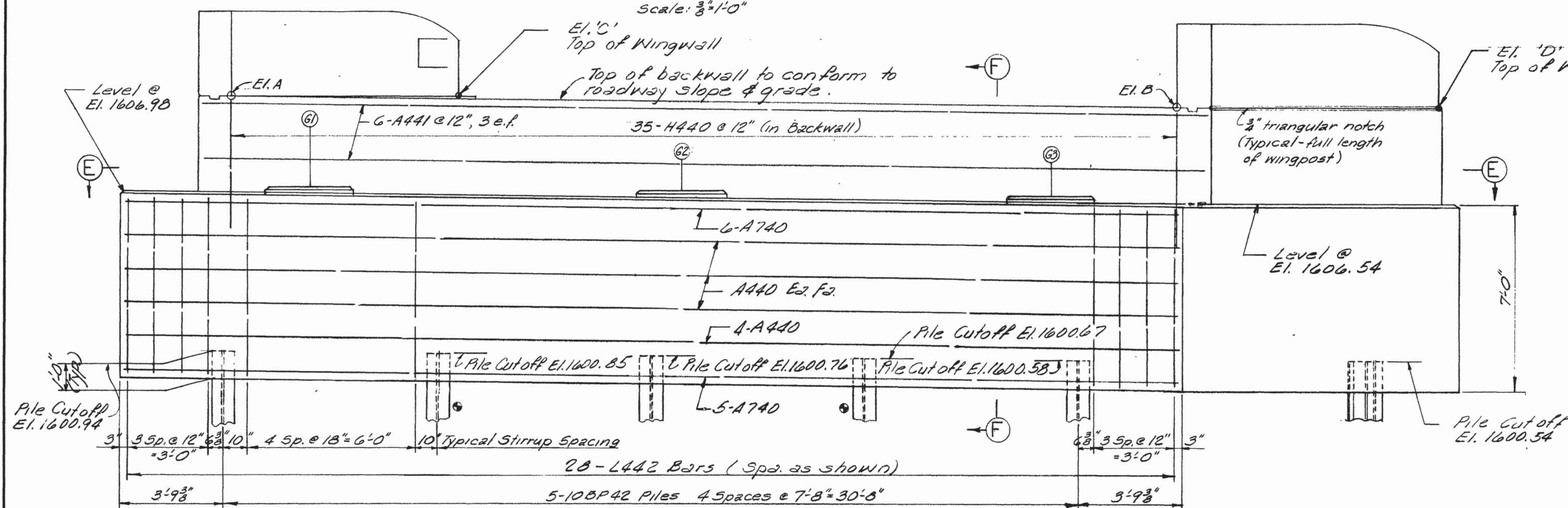
PIN NO.:	134991.00	
DESIGN BY:	SUSANNE DAWSON	DATE: MARCH 2024
DRAWN BY:	D. PICKEL	DATE: MARCH 2024
SUPERVISED BY:	SUSANNE DAWSON	DATE: MARCH 2024
CHECKED BY:	FRANK BALE	DATE: MARCH 2024



FED. ROAD DIST. NO.	T E N N	FEDERAL AID PROJECT NO.	FISCAL YEAR	SHEET NO	TOTAL SHEETS
3				89	31
FED. AID PROJ. NO.		APD-F-099-1 (6)			
STATE PROJ. NO.		82084-3205-64			



PLAN - ABUTMENT NO. 2
Scale: $\frac{3}{8}'' = 1'-0''$

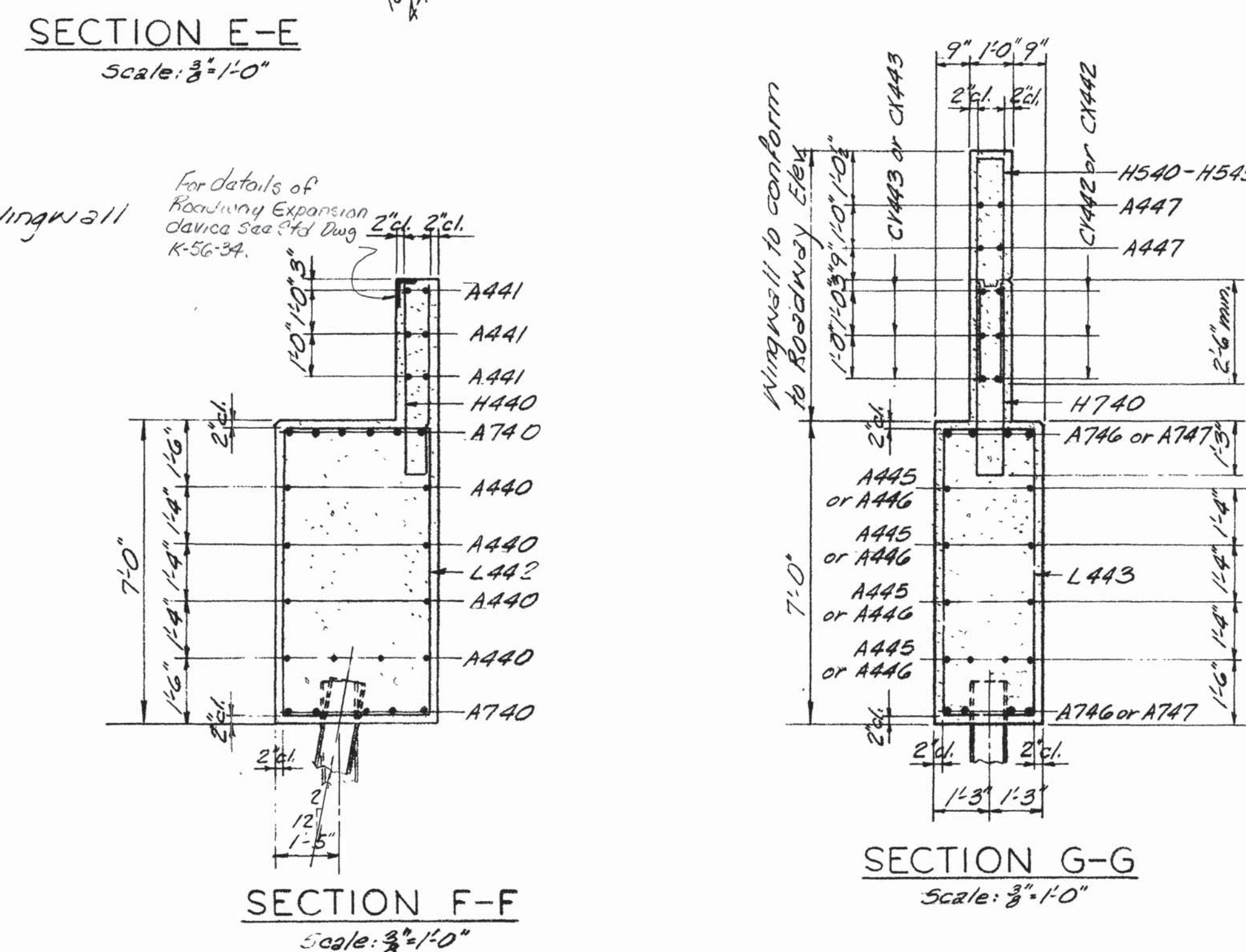


ELEVATION-ABUTMENT NO. 2
Looking Ahead on Survey
Scale: $\frac{3}{8}" = 1'-0"$

Note: Piles in wingwall not shown. See Section E-E for location. Cut off El. 1600.98

Note: ① Indicates batter Piles.
See Section F-F

Note: Top 1" of backwall to be poured concurrently with Superstructure.



SECTION G-G
Scale: $\frac{3}{8}'' = 1'-0''$

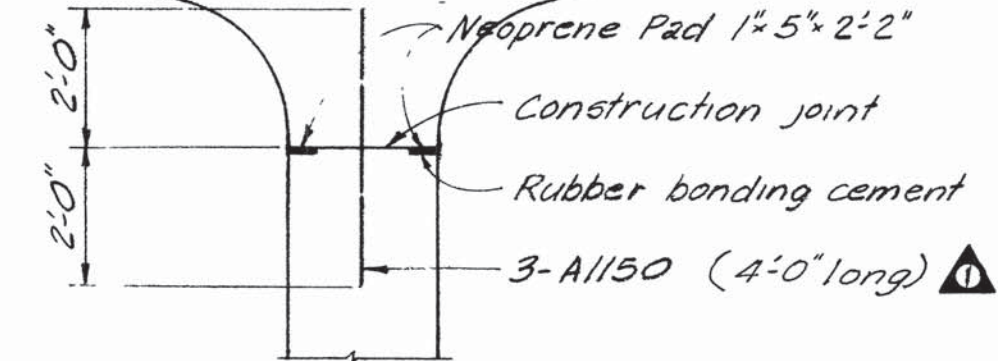
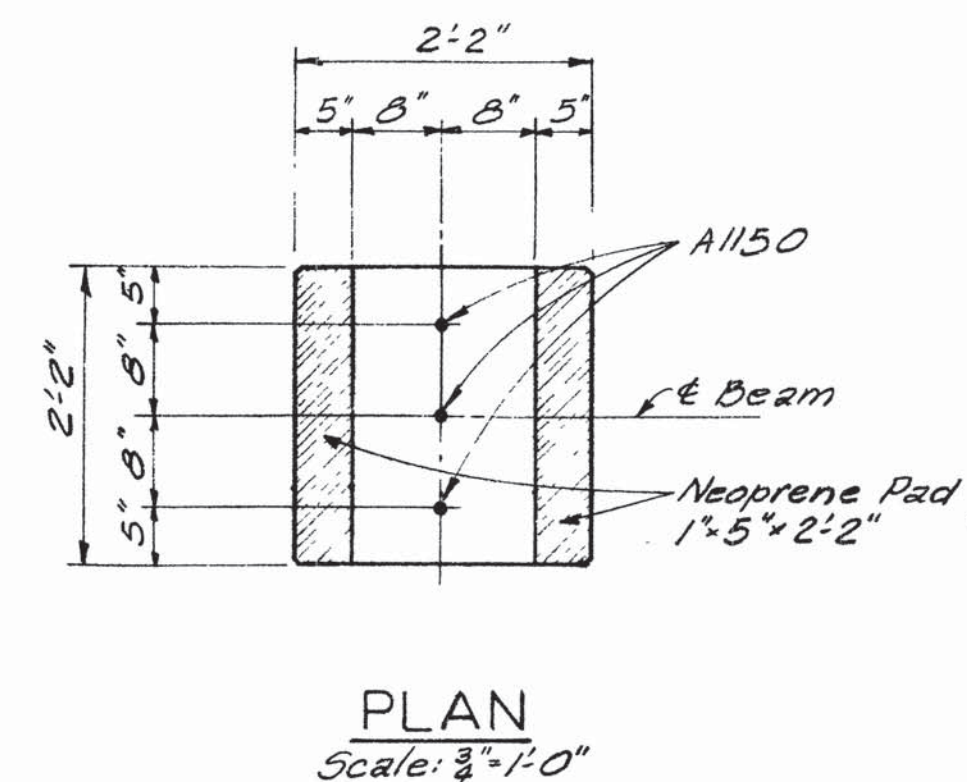
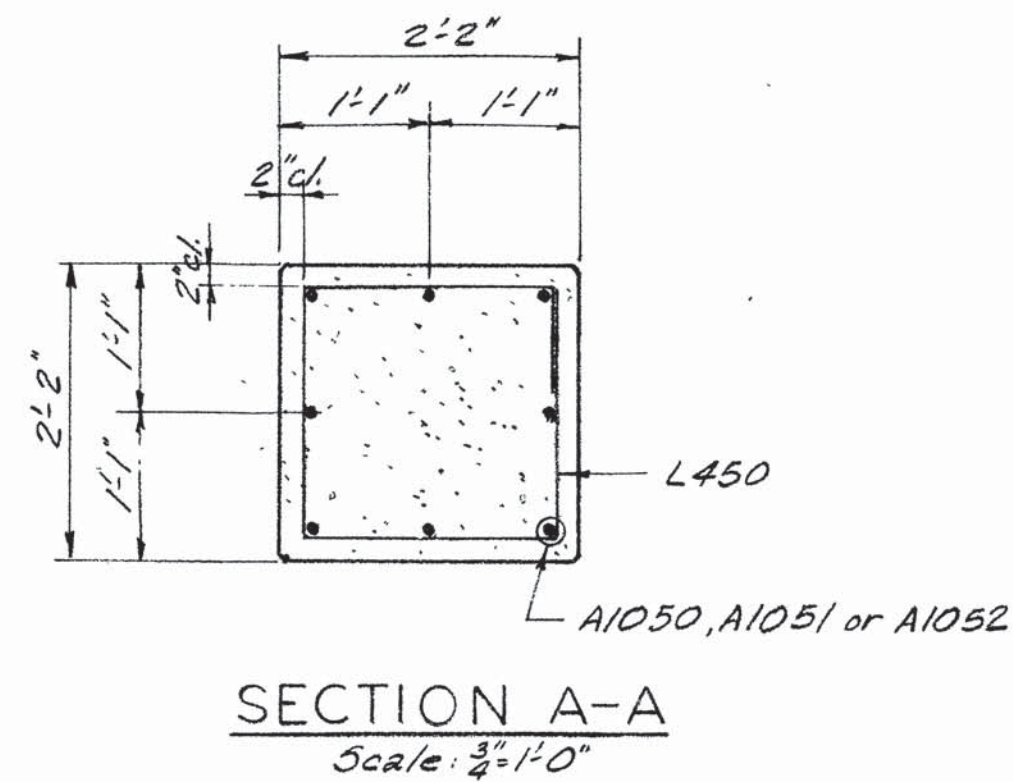
Note:
For Elevations, Quantities,
Wingpost Details and
Elevation H-H, see
Dwg. K-68-64

STATE OF TENNESSEE DEPARTMENT OF HIGHWAYS NASHVILLE			
STATE RTE. NO. 137 CROSS RD. OVER STATE RTE. NO. 137 AT STA. 325+25.00 SULLIVAN COUNTY, TENN. ABUTMENT NO. 2			
MID-SOUTH ENGINEERING CO., INC. 820 BROADWAY, N.E. KNOXVILLE, TENNESSEE			
SCALE $\frac{3}{8}'' = 1'-0''$	DESIGNED: DRAWN: JMM TRACED: CHECKED: S.M.K.	QUANTITIES COMP.: CKD:	DATE May 15, 1968

K-68-65

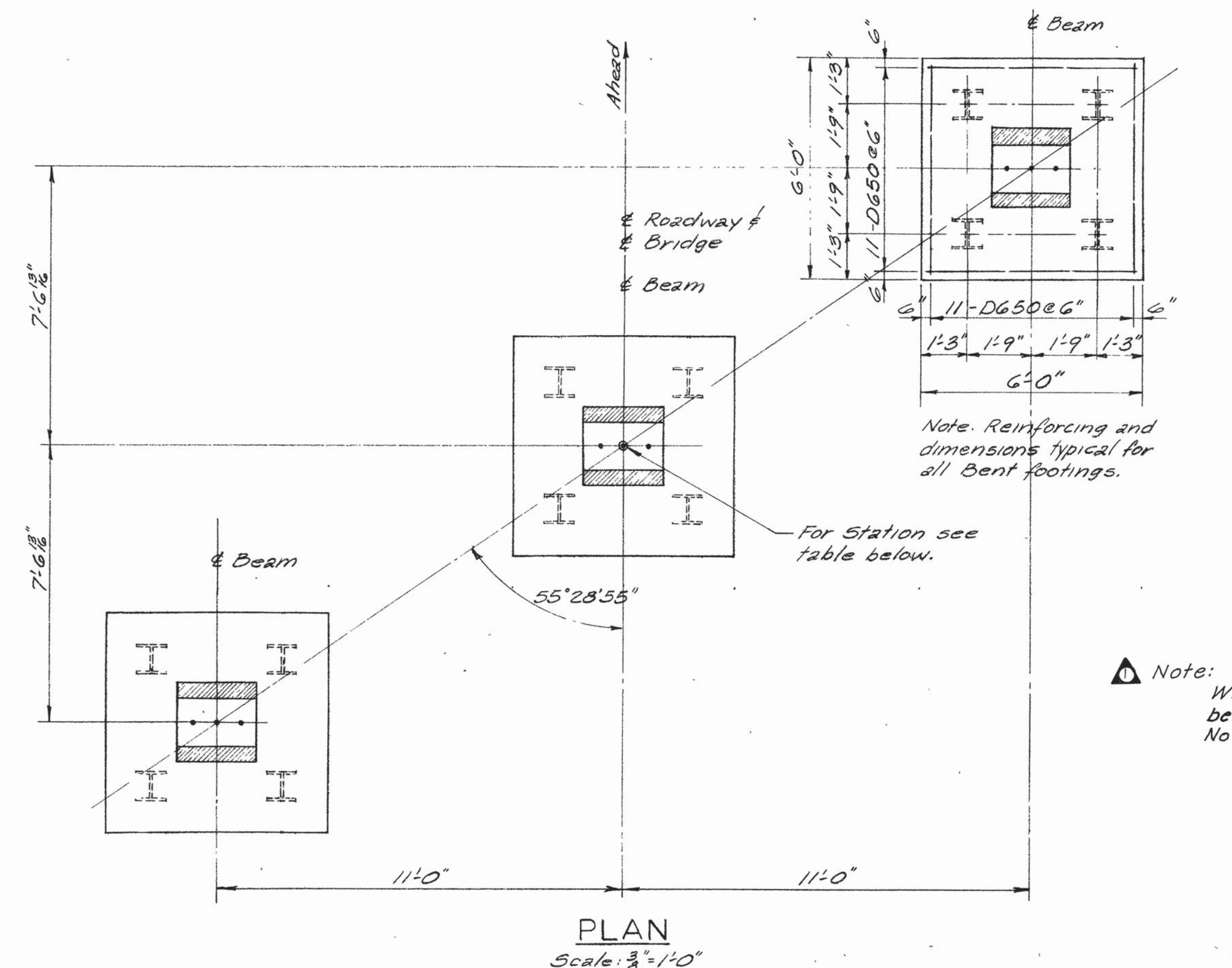
FED. ROAD DIST. NO.	TENN.	FEDERAL AID PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
3				90	312
FED. AID PROJ. NO.		APD-F-099-1 (6)			
STATE PROJ. NO.		82084-3205-64			

Revised 7-1-68 Changed Dim., Added Note

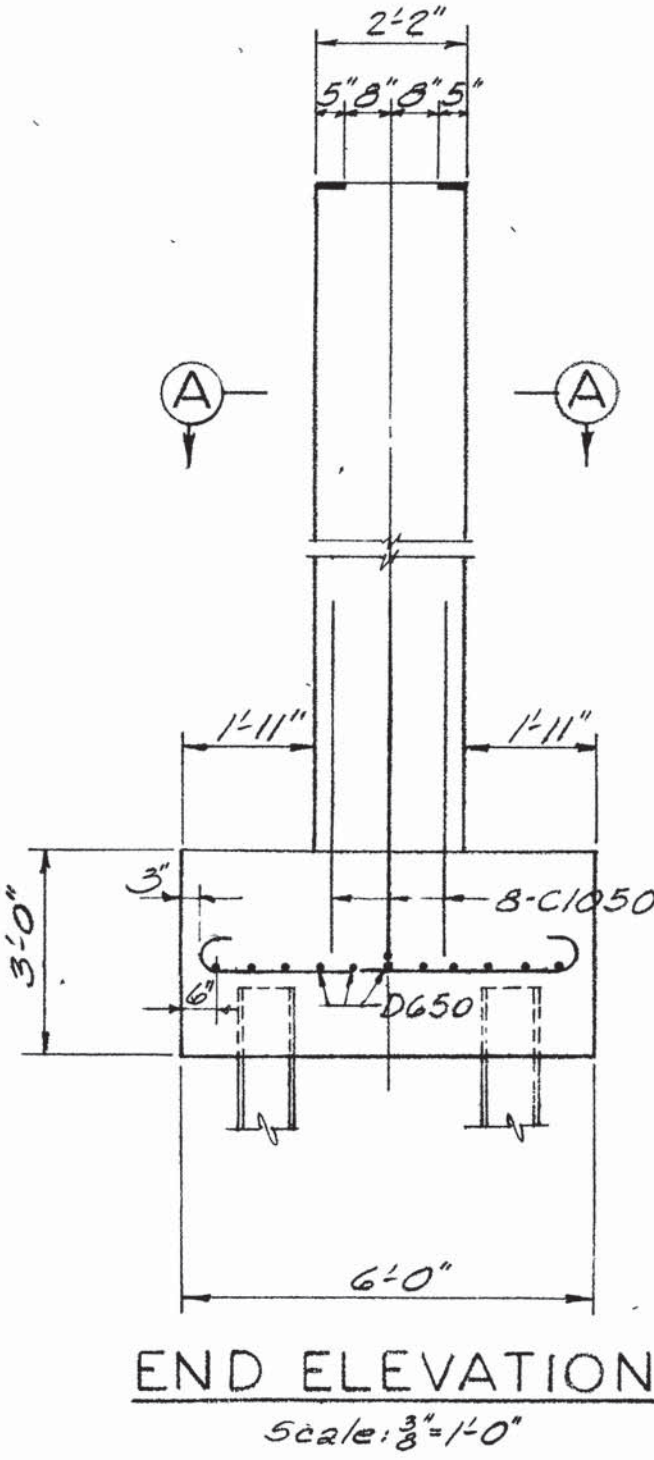
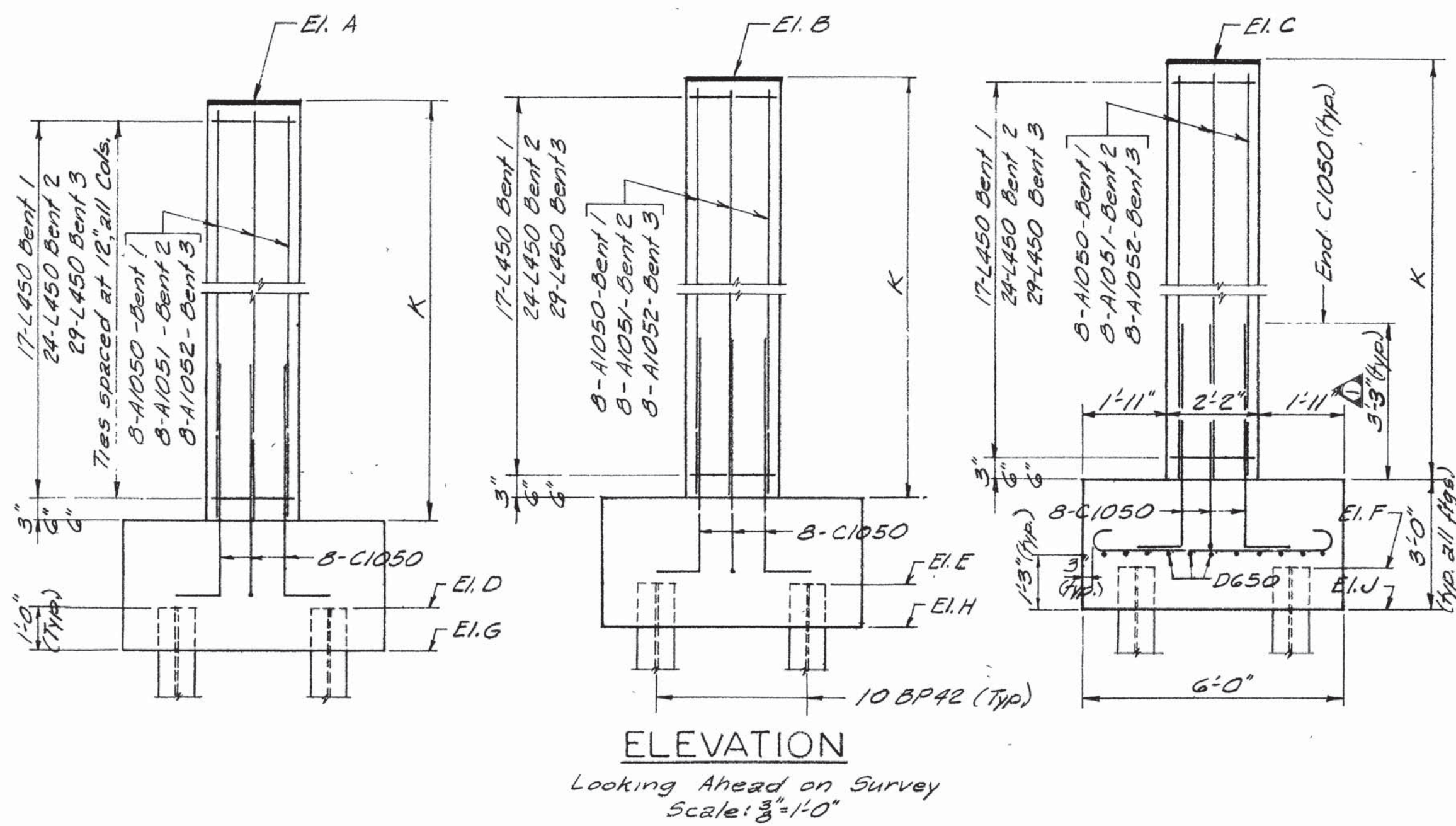


Note: Neoprene pads shall be in place a minimum of two days before being disturbed by placing forms for super-structure concrete. Place rubber bonding cement in such a way that column surfaces will not be stained.

ELEVATION
Scale: 3/8" = 1'-0"



Note: When pouring columns, provisions shall be made for setting dowel bars A1150. No chamfer on top of columns.



ESTIMATED QUANTITIES

Item	Unit	Bent 1	Bent 2	Bent 3
Class 'A' Conc.	Cu. Yds.	20.6	24.5	27.1
Reinf. Steel	Lbs.	3465	4352	4948

STATE OF TENNESSEE
DEPARTMENT OF HIGHWAYS
NASHVILLE

STATE ROUTE NO. 137
CROSS RD. OVER STATE ROUTE #137
AT STA. 325+25.00
SULLIVAN COUNTY, TENN.
BENTS

MID-SOUTH ENGINEERING CO., INC.
820 BROADWAY, N.E. KNOXVILLE, TENNESSEE

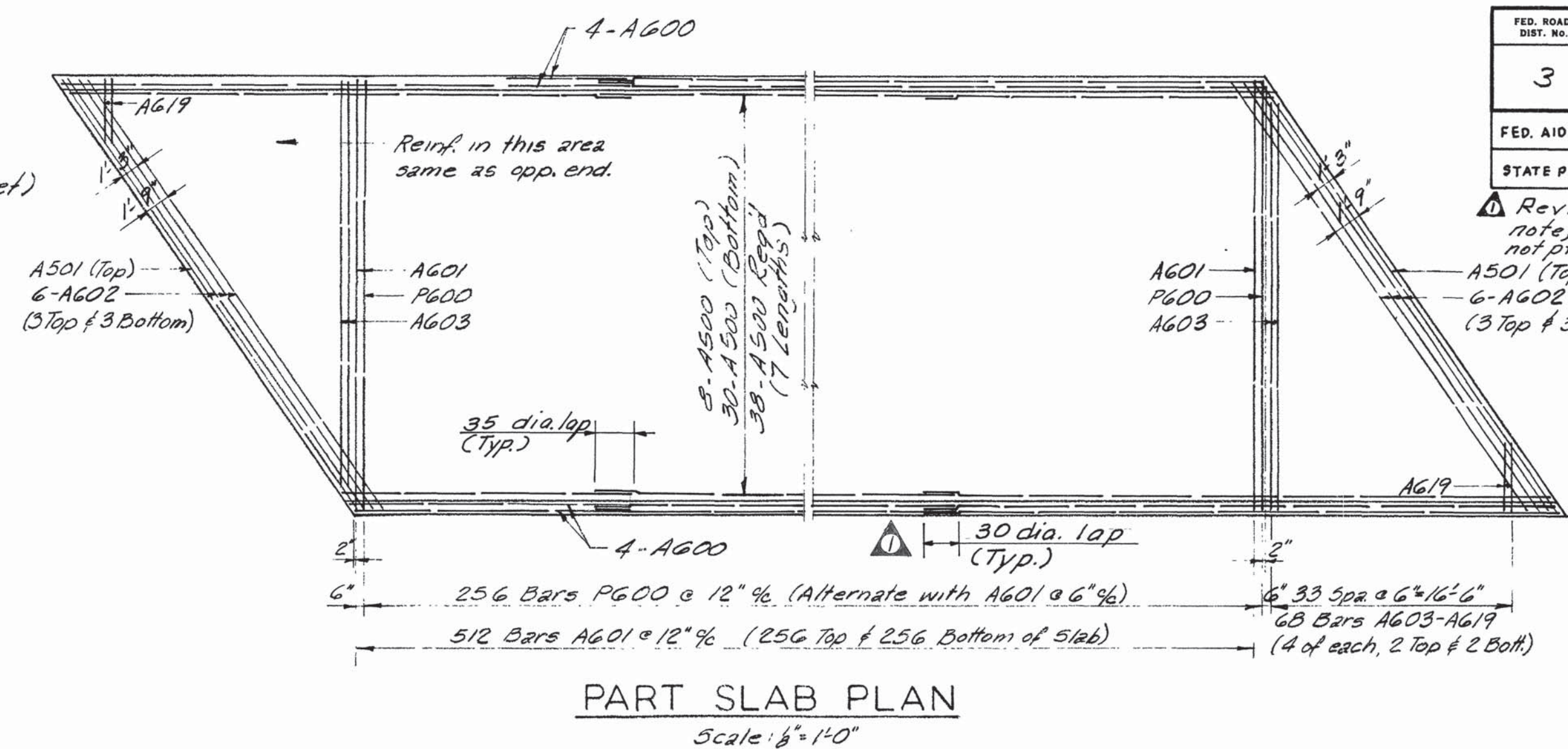
SCALE: As Noted
DESIGNED: SM-K
DRAWN: UMM
TRACED:
CHECKED: SM-K

QUANTITIES
COMP.:
CK'D:

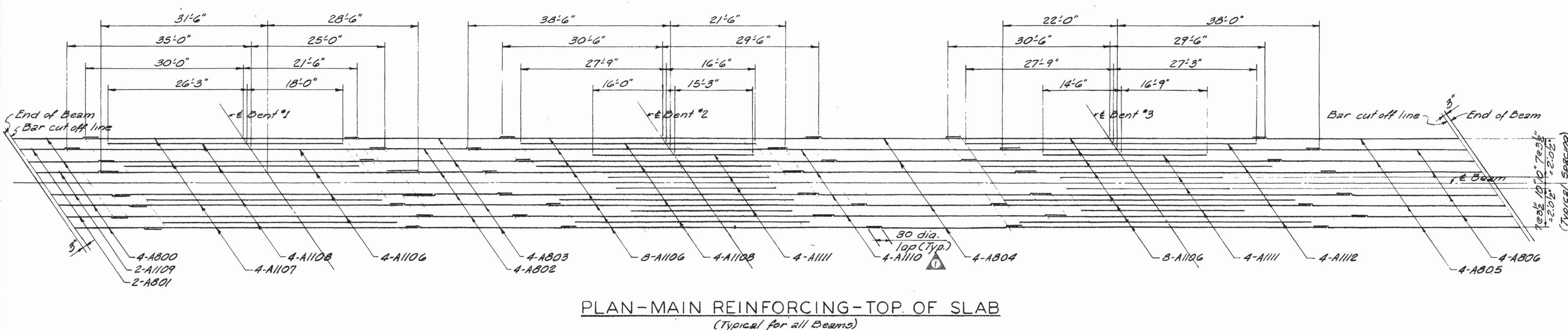
DATE
May 15, 1968

TABLE OF STATIONS, ELEVATIONS & DIMENSIONS										
ITEM	STATION	ELEVATION								DIM.
		A	B	C	D	E	F	G	H	
Bent 1	2+82.00	1592.78	1593.38	1593.74	1574.28	1574.88	1575.24	1573.28	1573.88	16'-6"
Bent 2	3+62.00	1597.30	1597.77	1598.00	1571.30	1571.77	1572.00	1570.30	1570.77	24'-0"
Bent 3	4+47.00	1600.72	1600.96	1601.06	1569.72	1569.96	1570.06	1568.72	1568.96	29'-0"

Revised 7-1-68 Added conc. placing note; steel splice note; showed re-steel not previously shown in cross section;
501 (Top) added dimensions to cross sec
6-AG02
3 Top & 3 Bottom)

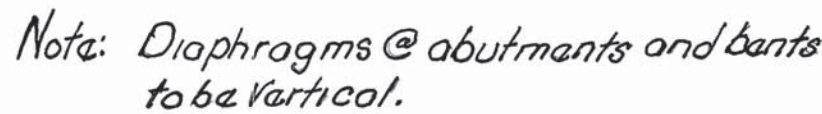


PART SLAB PLAN
Scale: $\frac{1}{8}'' = 1'-0''$

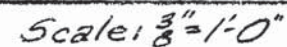
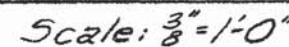
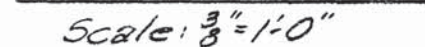
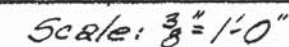
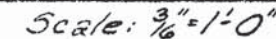
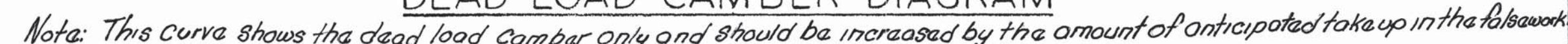
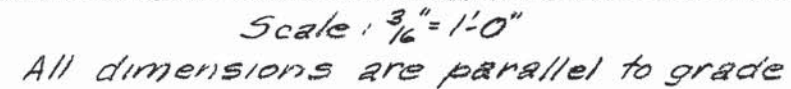


STATE OF TENNESSEE DEPARTMENT OF HIGHWAYS NASHVILLE			
STATE ROUTE NO. 137 CROSS RD. OVER STATE ROUTE #137 AT STA. 325+25.00 SULLIVAN COUNTY, TENN. SLAB DETAILS			
MID-SOUTH ENGINEERING CO., INC. 820 BROADWAY, N.E. KNOXVILLE, TENNESSEE			
SCALE As <i>Noted</i>	DESIGNED: P.F.H. DRAWN: JMMI TRACED: CHECKED: S.McK	QUANTITIES COMP.: CK'D.:	DATE <i>May 15</i> <i>1968</i>

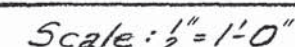
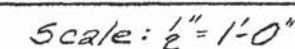
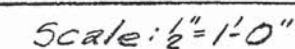
Revised 7-1-68 Added diaphragm note; Added detail expansion bearing to section.



Scale: $\frac{3}{16}'' = 1'-0''$
All dimensions are parallel to grade



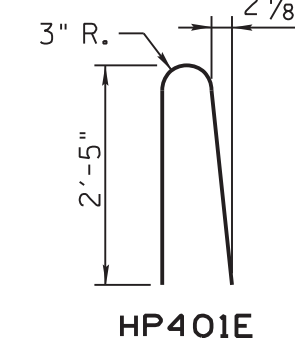
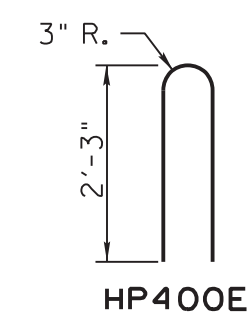
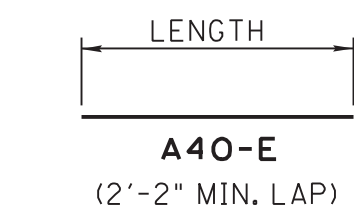
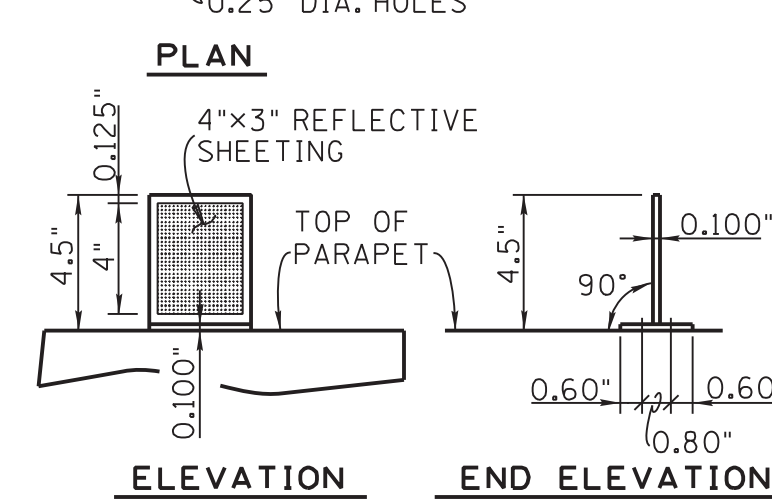
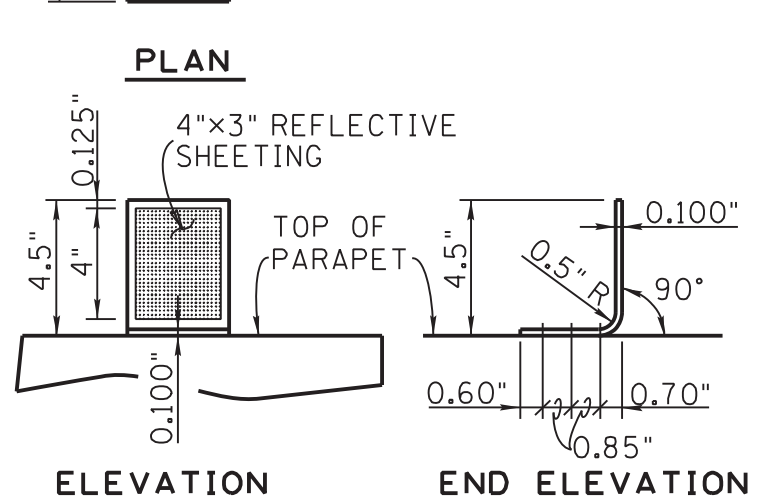
Scale: 1" = 1'-0"



Weight Each:
Structural Steel 40 lb.
Bronze 150 lb.

TABLE OF DIMENSIONS

STATE OF TENNESSEE DEPARTMENT OF HIGHWAYS NASHVILLE			
STATE ROUTE NO. 137 CROSS RD. OVER STATE ROUTE # 137 AT STA. 325+25.00 SULLIVAN COUNTY, TENN. SUPERSTRUCTURE DETAILS			
MID-SOUTH ENGINEERING CO., INC. 820 BROADWAY, N.E. KNOXVILLE, TENNESSEE			
SCALE <i>As Noted</i>	DESIGNED: <i>P.F.H.</i> DRAWN: <i>JMM</i> TRACED: CHECKED: <i>S MCK</i>	QUANTITIES COMP.: CK'D:	DATE <i>May 15, 1968</i>
		K-68-68	



1. BAR DIMENSIONS ARE OUT TO OUT. FIRST DIGIT OF THE NUMBER INDICATES SIZE.
2. THESE BARS SHALL BE FULL LENGTH OF PARAPET EXCEPT THAT NO BAR WILL PASS THROUGH OPEN JOINTS.

★ BARS A470E AND BARS B470E TO BE INCLUDED IN EPOXY COATED REINFORCING QUANTITIES AND BILL OF STEEL FOR WINGWALLS.
★ BARS B471E TO BE INCLUDED IN EPOXY COATED REINFORCING QUANTITIES AND BILL OF STEEL FOR SUPERSTRUCTURE.
NOTE: BARS LISTED IN ABOVE NOTES ARE NOT INCLUDED IN WINGPOST QUANTITIES OR PARAPET QUANTITIES SHOWN THIS SHEET.

DESIGN: AASHTO SPECIFICATIONS CURRENT EDITION WITH ADDENDA.

SPECIFICATIONS: STANDARD ROAD AND BRIDGE SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION. (CURRENT EDITION).

CONCRETE: TO BE CLASS 'A' $f'_c = 3,000$ psi. SEE SPECIAL PROVISION REGARDING SECTION 604 - CONCRETE STRUCTURES.

REINFORCING STEEL: TO BE ASTM A615 GRADE 60. SPACING DIMENSIONS ARE CENTER TO CENTER UNLESS OTHERWISE NOTED. THE SUFFIX 'E', FOR BARS SO MARKED, DENOTES EPOXY COATED REINFORCEMENT. SEE SPECIAL PROVISION 907A

(A) NOTE: CONTRACTOR MAY POUR THE PARAPET WITHOUT FELT PAPER PROVIDED HE FORMS A 1/2 INCH V-GROOVE ALONG THE TRAFFIC FACE AND TOP OF PARAPET DURING CASTING OF CONCRETE OR SOON AFTER SLIP-FORMING.

PARAPET DELINEATOR REFLECTIVE SHEETING SHALL MEET ASTM D4956, TYPE V SPECIFICATIONS. THE REFLECTIVE SHEETING SHALL BE A MINIMUM OF 4"x3" AND HAVE MINIMUM COVERAGE AREA OF 12 SQUARE INCHES. THE COLOR OF THE DELINEATORS SHALL CONFORM TO THE REQUIREMENTS OUTLINED IN MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. PARAPET DELINEATORS WILL NOT BE REQUIRED IN AREAS WHERE ROADWAY IS LIGHTED.

THE COST OF FURNISHING AND INSTALLING PARAPET DELINEATORS, INCLUDING ALL MATERIALS, LABOR AND INCIDENTALS NECESSARY TO COMPLETE THE INSTALLATION, SHALL BE INCLUDED IN BID PRICE FOR CONCRETE PARAPET.

PARAPET DELINEATORS SHALL BE MOUNTED TO THE CONCRETE PARAPET WITH A ONE COMPONENT ADHESIVE AS RECOMMENDED BY THE MANUFACTURER. THEY SHALL BE INSTALLED NO EARLIER THAN THREE WEEKS AFTER THE TEXTURE COATING HAS BEEN APPLIED.

1. OPEN JOINTS OR FILLED JOINTS WILL BE ALLOWED IN PARAPET ONLY WHEN SHOWN ON PROJECT DRAWINGS. JOINTS SHALL CONFORM TO THE JOINT DETAILS ON THIS SHEET OR AS OTHERWISE SHOWN ON PROJECT DRAWINGS.

2. PARAPET CONCRETE SHALL NOT BE CAST PRIOR TO REMOVAL OF ALL SUPERSTRUCTURE RELATED FALSEWORK.
3. ALIGNMENT AND PROFILE OF PARAPET SHALL CONFORM TO ROADWAY PROFILE AND GEOMETRY.
4. IF THE PARAPET IS SLIP-FORMED, ROUNDED EDGES WITH A $\frac{3}{4}$ INCH RADIUS MAY BE USED INSTEAD OF THE $\frac{3}{4}$ INCH CHAMFER AS SHOWN.
5. A $\frac{1}{2}$ INCH DEEP V-GROOVE ALONG THE TRAFFIC FACE AND TOP OF PARAPET SHALL BE FORMED DURING CASTING OF CONCRETE OR SOON AFTER SLIP-FORMING @ 10'-0" CENTER-TO-CENTER.
6. A $\frac{3}{4}$ "x4" FLAT DUMBBELL OR FLAT RIBBED P.V.C. (POLYVINYL CHLORIDE) WATERSTOP MAY BE USED IN LIEU OF THE 1 1/4" SLOPE AT EDGE OF SLAB. WATERSTOP MUST BE CONTINUOUS ENTIRE LENGTH OF PARAPET. RUBBER WATERSTOP MUST BE FACTORY FIELD VULCANIZED OR CEMENT SPLICED WITH THE AID OF PREMOLDED JUNCTIONS, AND/OR UNIONS.

(PER WING, BASED ON 11'-8" WINGPOST)

CLASS 'A' CONCRETE C.Y.	REINFORCING STEEL LB.
1.09	81

(PER LINEAR FOOT)

CLASS 'A' CONCRETE C.Y.	REINFORCING STEEL LB.
.1119	8

P.V.C. (POLYVINYL
CHLORIDE) WATERSTOP
SEE FABRICATION
NOTE 6. ~

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE RAILING
CONCRETE PARAPET
2021

CORRECT

D A Zmierzynski
ENGINEER OF STRUCTURES

SHEET 1 OF 13

STD-1-1

DESIGNED BY C.M. HILES DATE _____
 DRAWN BY KIM FRANKENFIELD DATE _____
 SUPERVISED BY C.M. HILES DATE _____
 CHECKED BY _____ DATE _____